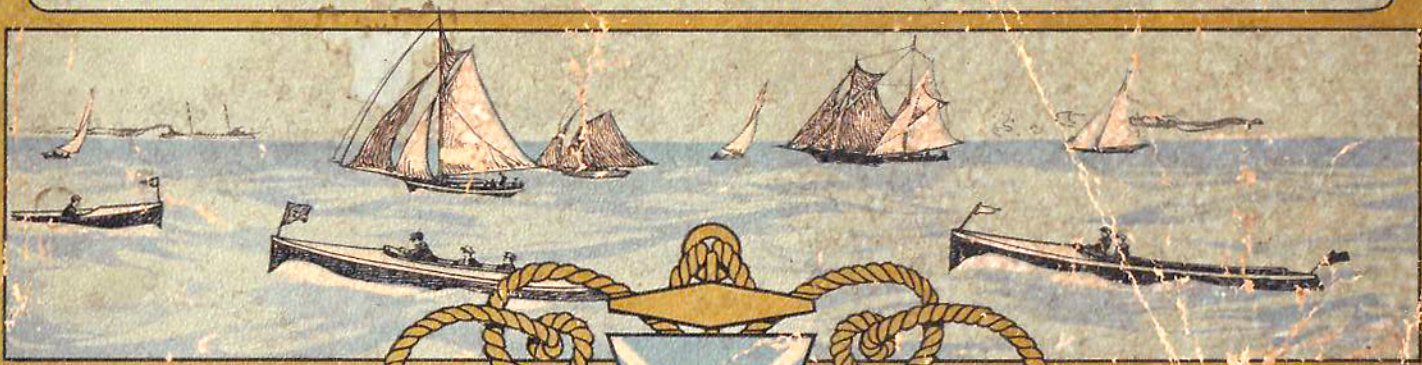


# INTER-LAKE YACHTING ASSOCIATION



1908

# WINTON



## The Exclusive 6—teen—6

- ☐ The only automobile in the world made in a plant that produces six-cylinder cars exclusively.
- ☐ The only **positive self-starting** six. Speed ranging from creeping to flying without shift of gears. Take hills on direct drive where fours fall back to second. Quiet, vibrationless, sweet running. Goes the route **like coasting down hill**.
- ☐ Precisely the car for the man who wants the best there is.

THE WINTON MOTOR  
CARRIAGE CO.

Member A. L. A. M.

CLEVELAND, O., U. S. A.

Branch Houses in

New York	Chicago	Philadelphia	Boston
San Francisco	Pittsburg	Seattle	Detroit
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# The Inter-Lake Yachting Association

## Year Book and P r o g r a m



Meet at

**Put-in-Bay, Ohio**

July 19th to 26th, 1908

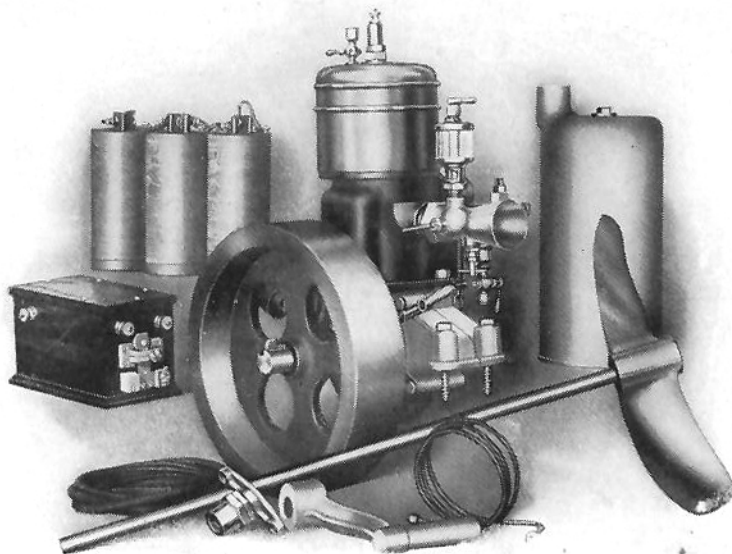
# REFINED MOTOR

## 3 H. P.

Bore  $3\frac{1}{2}$  inch

Stroke  $3\frac{1}{2}$  inch

All our thought,  
experience and  
skill devoted to  
the production of  
a REFINED  
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Immediate Ship-  
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Write for Catalog

Price of motor  
complete as shown

**\$50.<sup>00</sup>**

Including Boat  
Fittings

Manufactured by

## THRALL - FISHBACK MOTOR CO.

33 FORT STREET, EAST, DETROIT, MICH.

# The Inter-Lake Yachting Association

---

Cleveland, O., July 1, 1908

## GENERAL ORDER No. 1

The Fleet will rendezvous at Put in Bay on Sunday, July 19th.

Captains on arrival will report on board the flagship Priscilla and register. The anchorage will be in  
ht is provided with a berth; and when once located  
e meet.

les and regulations regarding yachting etiquette, the  
lagship at all times.

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GEO. H. WORTHINGTON,  
Commodore.

W. E. Scripps  
Pres.

Otto F. Barthel,  
Sec'y and Treas.

J. A. Pouliot  
Designer and Sup't.

## POULIOT BOAT CO.

Designers and Builders of

High Grade Cruising Yachts  
and Launches

MOTOR BOATS A SPECIALTY

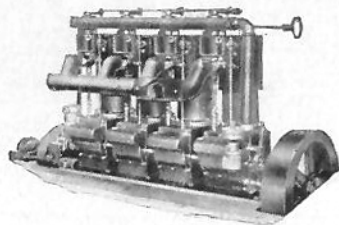
*Builders of*

36-ft. Autoboat "Unome" for Vice-Com. W. E. Scripps, D. M. B. C.

32-ft. Auxiliary Yawl "Bethlea" for W. H. Woods, D. Y. C.

53-ft. Auxiliary Schooner "Agawa" for Vice-Com. C. G. Jennings, I. L. Y. A.  
21-footer for Country Club Syndicate

DETROIT, MICH.



## "The Strelinger"

Let this motor  
run your launch

Only one cent an hour  
per H. P.

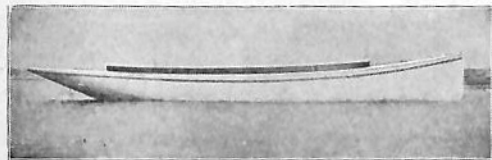
You start it in the morning and it will run till night, without any attention. Stop and start at your pleasure. Your power is always ready. Strong and steady. This motor will be a source of comfort and satisfaction to you. Send for catalog and learn more about our  
**STRELINGER ENGINES. 1 to 50 H. P.**

**THE STRELINGER MARINE ENGINE CO.**

Dept. Y. A. 46 Congress St. E.

DETROIT, MICH.

## J. W. HEPBURN Toledo, O.



Designer and  
Builder of

**Pleasure  
Boats**  
Power, Sail  
or Row

Anything from a 30-foot Cruiser to an 8 foot Dinghy.  
Seaworthiness or speed.

One Design Boats and Dinghys a Specialty

We built the TOLEDO YACHT CLUB one design Cats, winners in the Class  
of all Inter-lake and Inter-club races for three seasons

*Send for Catalogue*

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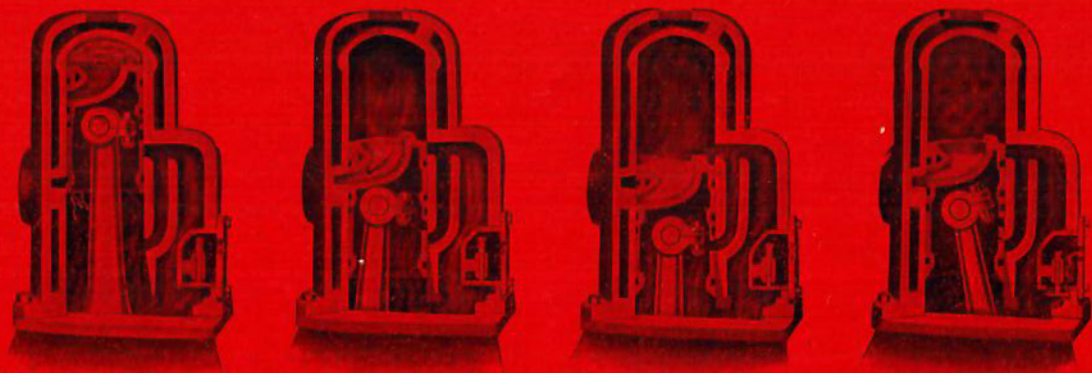
# You Want a "Claus" Motor



International Cabin Cruiser 43' x 8' Beam

Equipped with Claus Motor 4-Cylinder 5 $\frac{1}{2}$ ' x 6'

# You Want a "Claus" Motor



Pat. Dec. 4th, 1907

Two-Stroke Economy Fuel Consumption Less than that of the 4-Cycle

International Motor Co.

DETROIT, MICH.

Treasury Department  
Office of the Secretary

Washington, June 15, 1908

Mr. Geo. H. Worthington,  
Commodore Inter-Lake Yachting Association,  
Cleveland, Ohio.

Sir:—Referring to your communication of the 9th instant you are informed that the Revenue Cutter Morrill, stationed at Detroit, Michigan, will be detailed at the proper time, to enforce the regulations governing the regatta to be held at Put in Bay on the 20th of July next.

Respectfully,

H. S. MERRILL,  
Acting Chief of Division.

---

The celebrated Verdi Band of Cleveland, 28 performers, under the direction of Professor Pietro Oddo, will be in attendance throughout the meet and furnish music for all suitable occasions.



The  
**"UNOME"**  
 equipped with  
 a 6-cylinder Scripps

# Scripps Motors

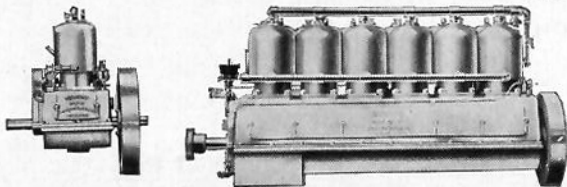
## 4-Cycle

**POWER.** Even running normal R. P. M., every engine we build **must** develop more horse-power than rated before leaving the factory. We guarantee more than rated power.  
**FUEL ECONOMY.** The fact that a 4-cycle motor uses less gasoline than a 2-cycle of the same size is unquestioned. The Scripps uses less gasoline than the average 4-cycle engine.  
**EASE OF OPERATION.** Primarily built for the novice as well as the expert. So simple your wife or boy can run it. Starts without cranking.  
**APPEARANCE.** Positively the handsomest motor on the market at any price—none excepted. Highly finished castings and all trimmings polished brass.  
**COST.** The Scripps is not "cheap" in a money sense; but fuel-saving and long life considered, it really costs less than any other engine on the market. It will outlast your boat. Write today for 1908 catalog. From 1 to 6 cylinder, 4 to 60 h. p.

**SCRIPPS MOTOR CO.**

Lincoln Ave.

Detroit, Mich.



# SCHUG BATTERIES

Oak Case. Nothing to Corrode—bottom fastened in with Maple Pins. Plugs, Timers, Switches, Spark Coils, Wire, etc.



**SCHUG ELECTRIC MFG. CO.**  
 320 Jefferson Ave.      DETROIT, MICH.

# PROGRAM

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## MONDAY, JULY 20

8:30 A. M. First sailing race. Classes A, B, C, F, H and J.

2:00 P. M. Dress ship. Review of the fleet by the Commodore and Staff.

At 2:15, accompanied by the Officers and Chairmen of Committees, the Commodore will leave the Flagship and pass through the lines. As the party passes each yacht will salute with one gun or by dipping colors. After the review a general reception for all members of Clubs and their ladies will be held on the Flagship and the boats of the Vice and Rear Commodores.

7:00 P. M. Banquet at Hotel Victory. Tickets can be procured from the Secretary-Treasurer and members of the Banquet Committee. Price, \$1.00.

## TUESDAY, JULY 21

8:30 A. M. Second sailing race. Classes A, B, D, F, H and J.

1:30 P. M. Power dinghy race. Rowing dinghy races.

3:00 P. M. Power boat races. Free-for-all race for all classes. See Power Boat Program, pages 21 to 27.

8:00 P. M. Receptions on all the yachts. The band will be stationed on a barge in the middle of the fleet.

A meeting of power boat men will be held at headquarters Tuesday evening at 7:30. At this meeting the Committee will endeavor to settle disputes and entertain suggestions for the betterment of the sport and advancement of the power boat interests in the Association. A full attendance is desired.

## WEDNESDAY, JULY 22

8:30 A. M. Third sailing race. Classes A, B, C, D, F, H and J.

1:30 P. M. Power boat races. Handicap races for all classes. See Power Boat Program, pages 21 to 27.

8:30 P. M. Grand Gala Night. Entertainment on board yachts. Brilliant illumination of the fleet. Paine's gorgeous fireworks. Chinese lanterns can be had of the Secretary-Treasurer, at small cost, and every yacht is expected to decorate.



## MOTOR BOAT ENTHUSIASTS

all agree that our

### “RED TOP” COLUMBIA IGNITOR

Dry Cell is a WINNER  
when it comes to  
SPARKING

Made in three sizes. 2½ x 6 in., 3 x 7 in., and 3½ x 8 in.

Sold by all Leading Dealers.

## COLUMBIA FLASHLIGHTS



MADE IN THREE STYLES

Absolutely safe to use around  
gasoline or any inflammable  
material



One should be on every MOTOR BOAT

If interested, send for special circular

**National Carbon Company, U. S. A., Cleveland, Ohio**

## Program—Continued

### THURSDAY, JULY 23

- 9:00 to 10:00 A. M. Concert by Verdi Band.
- 10:00 A. M. Squadron sail. Preliminary gun at 9:30 A. M. Watch the Flagship for signals.
- 2:00 P. M. Water sports.  
Tilting Tournament; Tail End Canoe Race; Obstacle Canoe Race; Man Overboard Canoe Race; 25-yard Swimming Race; 50-yard Swimming Race; Under Water Swimming Race.
- 9:00 P. M. Annual Ball, Hotel Victory. Two bands of music. Tickets can be procured from the Secretary-Treasurer and members of the Ball Committee. Price \$1.00.

### FRIDAY, JULY 24

- 8:30 A. M. Fourth sailing race. Classes C, F, H and J.
- 2:00 P. M. Baseball match, Toledo yachtsmen vs. Detroit yachtsmen.
- 8:30 P. M. Smoker at Hotel Victory and distribution of prizes. Ladies are invited.

---

## PRIZES AGGREGATING \$2500.00

### SAILING RACE PRIZES GIVEN FOR THE SERIES OF RACES

- Class A—Toledo Yacht Club, purse of \$200.00; Erie Yacht Club, purse of \$50.00, and \$50.00 additional added by the Association, \$300.00.  
First prize, \$150.00; second prize, \$90.00; third prize, \$60.00.  
Winner of series will receive Championship Flag.
- Class B—Country Club of Detroit, purse of \$100.00, and \$200.00 additional added by the Association, \$300.00.  
First prize, \$150.00; second prize, \$90.00; third prize, \$60.00.  
Winner of series will receive Championship Flag.
- Class D—Detroit Boat Club Yachtsmen, purse of \$75.00, and \$75.00 additional added by the Association, \$150.00.  
First prize, \$75.00; second prize, \$45.00; third prize, \$30.00.  
Winner of series will receive Championship Flag.

# *Lynch & Dorer*

*Attorneys-at-Law*

## **Patents and Patent Litigation**

208-215 Society for Savings

Cleveland, O.

Associate Offices: Washington, New York and all Foreign Countries

## **LAUNCH OWNERS!**

We guarantee to increase the speed of any launch from one to three miles per hour.

There are absolutely no restrictions to this guarantee. It matters not what propeller you have been using.

### ***The Bryant & Berry Speed Wheel***

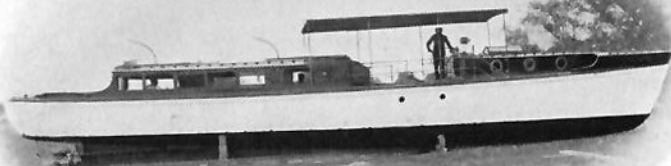
will do just what we claim or we will take it back at full price. The secret of the success of this wheel lies in its peculiar construction. The blades have a differential pitch and a concave curve from the hub to the outside of blade which holds water the whole length of blade, lessens the slip of the propeller, and insures the greatest possible speed development, and they cost no more than the ordinary kind. We also make all kinds of bronze fittings, fog-bells, etc.

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# The Matthews Boat Co.

Port Clinton, Ohio



Cruisers

Runabouts

Racing Craft

New Designs  
Advanced Ideas

Up-to-Date  
No Experiments

No Failures



## Program—Continued

Class F—Cleveland Yacht Club, purse of \$430.00.

First prize, \$215.00; second prize, \$130.00; third prize, \$85.00.

Winner of series will receive Championship Flag and Commodore Geo. W. Gardner Cup.

Class H—Lakewood Yacht Club, purse of \$225.00, and \$25.00 additional added by the Association, \$250.00.

First prize, \$125.00; second prize, \$75.00; third prize, \$50.00.

Winner of series will receive Championship Flag.

Class J—Buffalo Yacht Club, purse of \$70.00; Monroe Yacht Club, purse of \$25.00, and \$30.00 additional added by the Association.

First prize, \$63.00; second prize, \$37.00; third prize, \$25.00.

Winner of series will receive Championship Flag.

Class C—Maumee River Yacht Club, purse of \$25.00, and \$75.00 additional added by the Association.

First prize, \$50.00; second prize, \$30.00; third prize, \$20.00.

Winner of series will receive Championship Flag.

### POWER BOAT PRIZES

Class A—Sandusky Yacht Club, purse of \$50.00, and \$100.00 additional added by the Association, \$150.00.

Class B—Buckeye Lake Yacht Club, purse of \$25.00, and \$50.00 additional added by the Association, \$75.00.

Class C—I-L. Y. A., purse \$300.00.

*For division of Power Boat prizes, see Power Boat Program, pages 21 to 27.*

### DINGHY PRIZES

Power Dinghy Race, I-L. Y. A., purse of \$25.00.

First prize, \$12.50; second prize, \$7.50; third prize, \$5.00.

Professional Rowing Dinghy Race, I-L. Y. A., purse of \$25.00.

First prize, \$12.50; second prize, \$7.50; third prize, \$5.00.

Amateur Rowing Dinghy Race, I-L. Y. A., purse of \$25.00.

First prize, \$12.50; second prize, \$7.50; third prize, \$5.00.

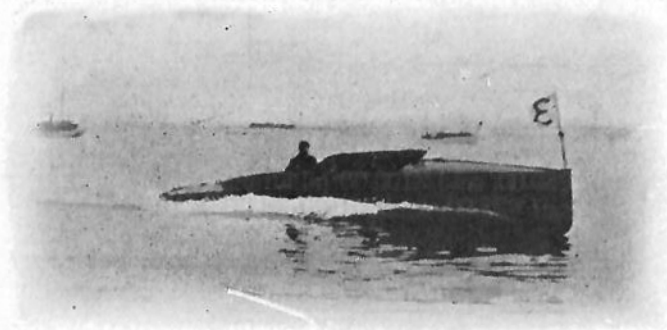
### OTHER SPORTS PRIZES

Commodore Geo. W. Gardner, purse of \$50.00, and \$75.00 additional added by the Association, \$125.00; from which the prizes for each event were secured as per program under title of "Other Sports."

# DETROIT LAUNCH & POWER CO.

DESIGNERS AND BUILDERS OF  
MOTOR BOATS, SPEED AND CRUISER TYPES  
YACHTS, YAWLS AND TENDERS

Repairing and Remodeling done Reasonably and Promptly. Two sets of Marine Railways in connection.  
Motors Installed and Blacksmithing. 21' to 30' Stock Boats on hand.



" COMET "

28' x 4' 6" Speed, 24.7 miles per hour actual. Champion of 1907.

" POINCETTA "— 34' x 7', Cabin Cruiser. 13.8 miles per hour. Champion of 1907. Built by us.

We Guarantee Up-to-Date Designs and Construction, and all the Speed there is in it.

Ft. Holcomb Ave., Detroit, Mich.

THE BEER OF CLEVELAND

*“Leisy’s”*

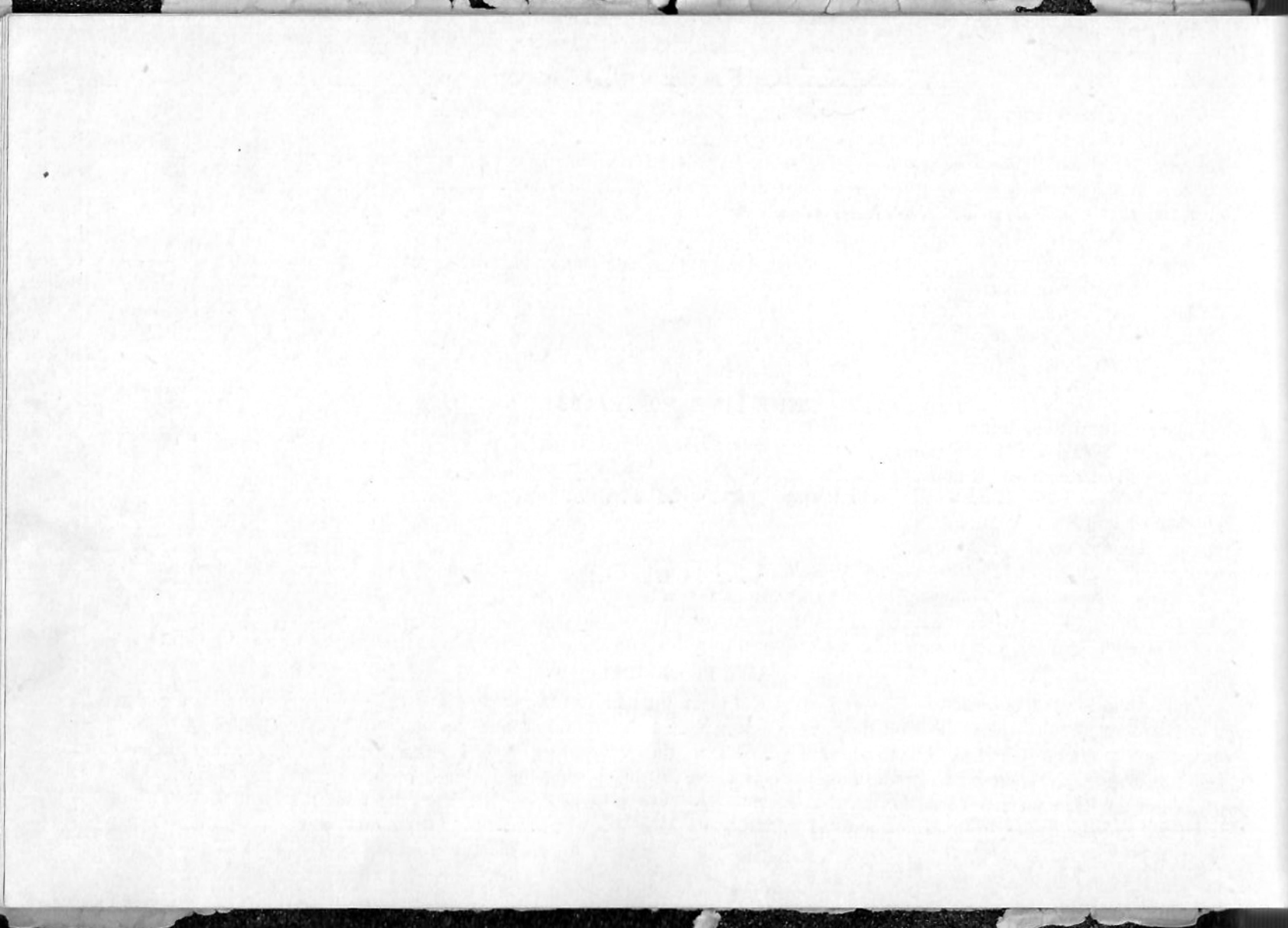
“SPECIAL BREW,” “PREMIUM,” “EXTRA PALE”

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OTTO I. LEISY, Mgr.



## SAILING RACE PROGRAM

Classes A and B will race on Monday, Tuesday and Wednesday, July 20, 21 and 22.

Class D will race on Tuesday and Wednesday, July 21 and 22.

Classes F, H and J will race on Monday, Tuesday, Wednesday and Friday, July 20, 21, 22 and 24.

Class C will race on Monday, Wednesday and Friday, July 20, 22 and 24.

### The sailing yachts will be divided into classes as follows:

Class A—Modern cruisers which measure, under the universal rule, 33 rating and over.

Class B—Modern cruisers which measure, under the universal rule, under 33 rating.

Class D—Veteran cruisers.

Class F—21 footers.

Class H—18 footers.

Class J—16 footers.

Class C—Cat-boats.

### TIME LIMIT FOR RACES

Classes A and F—3 hours.

Classes B, D, H and J—3½ hours

Class C (short course)—2 hours.

### STARTING TIME FOR SAILING RACES

Preliminary Gun, 8:00 A. M.

Preparatory Gun, 8:20 A. M.

Starting Gun, 8:30 A. M., 21-foot class; 8:35 A. M., Class A; 8:40 A. M., Class B; 8:45 A. M., 18-foot class; 8:50 A. M., 16-foot class; 8:55 A. M., Class D; 9:00 A. M., Cat-boats.

Five minutes between classes.

The starting gun for each class will be the preparatory gun for the next.

### SAILING COURSE

The course for sailing yachts in Classes A to J will be triangular, starting from a line between a stake-boat off the east end of Gibraltar and the judges' boat off the west end of Middle Bass, to and around a stake-boat 3 nautical miles N. N. W., thence to and around a stake-boat 3 nautical miles S. W.  $\frac{3}{4}$  S., thence 3 nautical miles E.  $\frac{1}{2}$  S. to and across starting line between judges' boat and stake-boat. In finishing, yachts must cross the starting line in the opposite direction from which they start. All stake-boats must be passed to port. A special course for cat-boats, starting from the same line, will be a shorter triangle with same compass courses, but with 1½ miles to the side.

Cent. 7385-R

Established 1888

Main 474

STRAN'S PATENT SKYLIGHT

**C. W. STRAN**  
*Sheet Metal Contractor*

**FIRE PROOF METAL WINDOW FRAMES**

In accordance with Specifications of National  
Board of Underwriters

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CLEVELAND, OHIO

We can save You all kinds of money on

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Old Town Canoes, priced at \$29.00 to \$47.00  
Indian Canoes, priced from \$31.00 to \$43.00  
Hudson River Canoes, priced from \$32.00 to \$48.00  
Racine Canoes, priced from \$31.00 to \$39.00  
Power Canoes, priced at \$150.00  
Launders Canoes, priced from \$94.50 to \$475.00  
Row Boats, priced from \$32.00 to \$65.00

*The May Co.*  
Cleveland, Ohio

**EVERYTHING FOR THE YACHTSMAN**

Whatever you need in the way of apparel, clothes, caps and furnishings, you can find it here, in the regulation style. We can make for you, too, any special style of uniform, with caps to match. We even go so far as to embroider your yacht club emblem on your shirt sleeve if you want it.

*Bennet & Fish*  
301-303-305 Euclid Avenue

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## Program—Continued

**Alterations** of the race program may be made at the discretion of the Regatta Committee and all changes will be posted one hour before each race.

**Yacht Racing Union** rules will govern for Classes A and B.

Class C will be raced under the following rule:

$$\text{RACING LENGTH} = \frac{\frac{1}{2}\sqrt{\text{SAIL AREA}} + \text{OVERALL LENGTH}}{2}$$

Sail area will be measured in accordance with the rules given in the 1904 hand-book of I.-L. Y. A., i. e., actual area of sails aft of the spar, and area of the forward triangle. Winners in Class C will be determined by use of the Yacht Racing Union table of time allowances.

**I.-L. Y. A.** rules will govern for 21-foot, 18-foot, 16-foot and Cat-boats.

## POWER BOAT CLASSES

**Class A—Cruisers.** All launches having permanent cabins and cruising accommodations and equipment, propelled solely by machinery. They shall race in cruising trim but are not required to carry dinghys and extra deck gear. It is not intended to include large steam yachts nor to bar other gasoline craft that come within the accepted definition of the class.

**Class B—Open Launches.** All launches not included in Class A and whose speed is not less than 7 nor more than 14 miles an hour.

**Class C—Speed Boats.** All open launches intended primarily for racing purposes, whose speed exceeds 14 miles an hour.

**NOTE.**—Should a boat owner fail to enter his boat in the proper class, he may either be reclassified for the handicap races or disqualified at the discretion of the committee. In either event he cannot win a place or prize in the free-for-all race.

## POWER BOAT PROGRAM—TUESDAY, JULY 21, 1908

**Free-for-all race for all classes.**

Course, 10 statute miles, 5 miles from starting line between Gibraltar and Middle Bass Islands to and around flag buoy and return (see bulletin board for chart of course and sailing directions).

# RALACO

## The Silent Simplified Engines

have been winners of all reliability and long distance races in which they have been entered.

Two Cycle - - 3 to 14 HP.

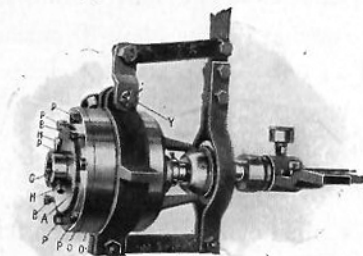
Four Cycle - - 10 to 50 HP.

### The S. M. Jones Co.

603 Segur Ave., TOLEDO, O.

## The "Paragon" Reverse Gear

The  
Most  
Reliable  
Reverse  
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Ever Used  
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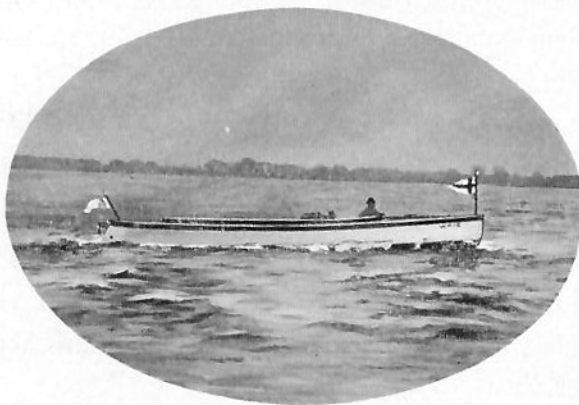
Manufactured by  
**Evans Stamping & Plating Co.**  
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U. S. A.

# Niagara Motor Boat Co.

200 Sweeney Street  
NORTH TONAWANDA, N. Y.

Builders of  
HIGH GRADE PLEASURE BOATS



### OUR SPECIALTY

Racing and Semi-speed Boats

Builders of the "Niagara", fastest boat on the Niagara River, with record of 25 miles per hour, 30 h. p. motor.

Write for catalogue or call at our factory when on your way to Niagara Falls. We are located at North Tonawanda, N. Y., which is between Buffalo and Niagara Falls on the N. Y. C. & H. R. R. R. and Buffalo and Niagara Falls trolley line.

## Program—Continued

Class A—Preparatory Gun, 3:00 P. M. Starting Gun, 3:15 P. M.

Class B—Starting Gun, 3:30 P. M.

Class C—Starting Gun, 4:30 P. M.

### PRIZES:

Class A—First, \$21.00; second, \$18.00; third, \$15.00; fourth, \$10.00; fifth, \$6.00.

Class B—First, \$12.00; second, \$9.00; third, \$6.00; fourth, \$3.00.

Class C—First, \$45.00; second, \$36.00; third, \$27.00; fourth, \$20.00; fifth, \$14.00; sixth, \$8.00.

Important meeting of power boat men at headquarters Tuesday evening at 7:30.

## WEDNESDAY, JULY 22, 1908

**Handicap races for all classes.** Handicaps to be based upon performance in free-for-all races on Tuesday.

Class A—Preparatory Gun, 1:30 P. M. Starting Gun for limit boat, 1:45 P. M. Course, 30 miles, from starting line between Gibraltar and Middle Bass islands to and around West Sister Island and return.

Class C—Time and order of starts to be posted on bulletin board. Course, same as on Tuesday, 10 miles.

Class B—Time and order of starts to be posted on bulletin board. Course, same as on Tuesday, 10 miles.

Class C—2nd Race. Time and order of starts to be posted on bulletin board. Course, same as on Tuesday, 20 miles.

### PRIZES:

Class A—First, \$40.00; second, \$25.00; third, \$15.00. Championship flag to winner.

Class B—First, \$22.00; second, \$14.00; third, \$9.00. Championship flag to winner.

Class C—First, \$75.00; second, \$45.00; third, \$30.00. Championship flag to winner. Highest average for two races.

## POWER BOAT RACING RULES

1. Competing boats must be enrolled in one of the recognized clubs belonging to the Inter-Lake Yachting Association, or especially invited to participate in the 1908 regatta. The latter applies to clubs of the Lake Michigan Yachting Association, and the Lake Yacht Racing Association of Lake Ontario.



Yacht "GRAYLING"

## Fastest Cruising Motor Boat in the World

of its length and beam, is equipped with three 40 h. p., 4-cylinder, Gray Motors (120 h. p.); winner of the Time Prize in the 200-mile race on Lake Erie, Aug. 11, 1907—write for interesting story of the race. But these very engines with which the "Grayling" made the fastest time are not one bit better than our \$67.50—2½ h. p. engine. Every Gray Motor must be absolutely perfect before it leaves our factory.

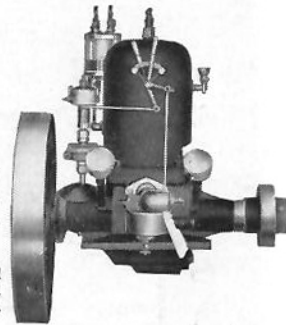
**6 h. p. Complete Boat Outfit \$97.50**

(Not Incl.) Shaft, Propeller, Wheel, Stuffing Box, Muffler, Batteries, Spark Coil, Wire, Switch, etc.

Our enormous 5 story factory is the largest and most modernly equipped plant in the world devoted exclusively to manufacturing 2-c, etc Marine Motors.

**2½ h. p. guaranteed to develop 3½ h. p. Everything complete, ready to install in your boat, except gasoline tank and piping. Is as powerful as most 4 h. p. motors. \$67.50**

Gray Engines are high grade in every detail of motors and outfit regardless of the low prices. Get description of the 1908 motors—2½ to 40 h. p., 1, 2, 3 and 4 cylinder.



Write for 1908 Catalogue today

**GRAY MOTOR CO., 68 Leib Street, DETROIT, MICH.**

## Loving Cups

We have furnished many of the "I. L. Y. A." Loving Cups, including the "Commodore Gardner Cup" and many others. Also the famous "Bowlus Cup" and long distance race cups for the T. Y. C. We carry the largest line in Ohio and will also be glad to furnish and figure on special designs.

**The Merrill & Broer Co.**  
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## Dingfelder Motor Co.

DETROIT, MICH.



BUILDERS OF HIGH GRADE  
**HEAVY DUTY ENGINES**

## Program—Continued

2. Full direction and control of the races rests with the Power Boat Racing Committee and all questions and disputes shall be decided by it.

3. No entry fee will be charged.

4. Entry blanks may be had at headquarters and must be filled out by the owner or a credited representative of the entering boat. No entry will be received later than 12 o'clock noon, on Tuesday, July 21.

5. Competing boats must carry racing numbers to be furnished by the committee upon acceptance of entry. These numbers must be carried on the forward flag staff.

6. Changes of schedule and all special notices will be posted on the bulletin board at headquarters. Boat owners are expected to consult the bulletin board not more than one hour before the scheduled start of each race, as weather conditions and unforeseen contingencies may arise to alter the printed program. If this request is overlooked there will be no appeal.

7. Handicaps for the second day's racing will be based upon performance in the free-for-all races. A boat which exceeds her rated speed by more than 3 per cent shall be disqualified.

8. The championship in each class will be decided in the handicap races. To qualify for the handicap races it will be *absolutely necessary* for a competing boat to *start in the free-for-all race* for her class.

9. There shall be a championship flag for the boat in each class which secures the highest per cent of rating in the second day's races without exceeding the 3 per cent penalty.

10. The championship in the speed boat class will be awarded the boat making the highest total per cent rating in the two handicap races. Should a speed boat exceed her rated speed by 3 per cent in the first handicap race, she shall be disqualified for that race, but may start in the second race upon a new rating based upon her performance in the previous race.

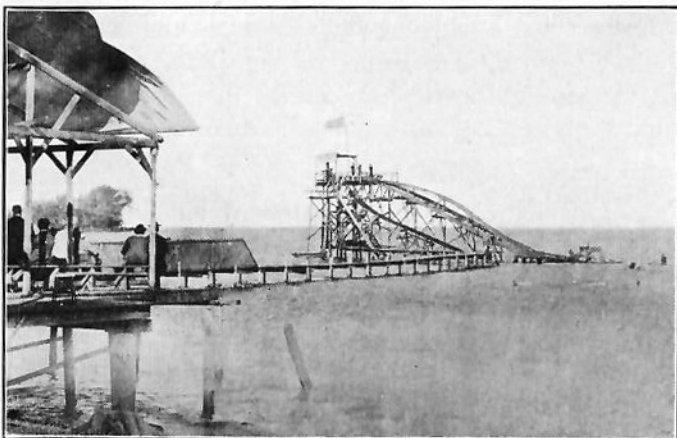
11. One hour previous to the race, the Club bulletin board will display the sequence of starts in each class, the order of the classes, and the time of start for each boat.

12. When the preparatory gun is fired, a colored flag will be displayed on the Judge's boat, which will designate the first class to start, as follows:

Red—Class A—Cruisers.

White—Class B—Open Launches.

Blue—Class C—Speed Boats.



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Free Steam Toboggan Slide and Water Chutes

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Toledo, Ohio

## Program—Continued

13. As soon as a class is over the line, the flag will be changed to the color designating the class to follow next.

14. All starts shall be flying and one gun starts. Any boat which has crossed the line before the signal for her start, must turn and recross the line, keeping clear of all others. Throughout the race, all boats must observe the "Rule of the Road." Boats must keep at least 100 yards back of the starting line until the boat ahead of them has been sent away. This rule is intended to prevent accident and interference.

15. All turning marks shall be left on the port side, unless special directions to the contrary are posted.

16. Every boat shall render all possible assistance to any other vessel or person in peril. If, in the judgment of the committee, she shall thereby have impaired her chance of winning, they may order the race restarted between such boat or boats and the winner in the class.

17. An overtaking boat must keep clear of the boat overtaken when overlapping, except in rounding stakes or buoys, when the outside boat, or the one overtaken, must make way to allow the one behind to clear the mark, provided there is an overlap. An overlap is established when an overtaking boat has no longer a free choice on which side she will pass.

18. Written notice of protest must be handed personally to the chairman of the race committee within six hours after race and must be signed by the owner or accredited representative of the boat protesting.

19. All power boats shall be provided with some mechanical means approved by the racing committee, by which the propulsive action of the propeller wheel can be reversed without bringing the engine to a state of rest for the purpose of reversing same.

## OTHER SPORTS

### General Announcement

These events will be under the supervision of the Committee on Other Sports.

#### Entries

All entries must be in the hands of the Other Sports' Committee at Headquarters not later than 7 P. M. Wednesday, July 22, 1908.

#### Prizes

All prizes in gold and bronze are of special design and are particularly appropriate for these events.

#### Changes

The announcement of any change in the program and all further details will be posted on the bulletin board at headquarters.

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**PEPPERMINT  
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**CHEW  
WHITE'S  
YUCATAN  
GUM.**

**CHEWING  
GUM.**

*To relieve the distressing pains of*  
**DYSPEPSIA and HEARTBURN**

**THIS IS THE LARGEST SELLER IN THE WORLD**



## Program—Continued

### PROGRAM

Thursday, July 23, 1908. 2 P. M.

#### Tilting Tournament

Winning Spearsman—Gold Watch Fob.

Winning Paddler—Gold Watch Fob.

#### Tail End Canoe Race

First Prize—Extra Heavy Sweater.

Second Prize—Good Sweater.

#### Obstacle Canoe Race

First Prize—Double Canoe Air Cushion.

Second Prize—Single Canoe Air Cushion.

#### Man Overboard Canoe Race

A handsome gold charm to each member of the winning team.

#### 25 Yard Swimming

First Prize—Extra Fine Woolen Bathing Suit.

Second Prize—Fine Woolen Bathing Suit.

#### 50 Yard Swimming

First Prize—Gold Cuff Buttons.

Second Prize—Gold Scarf Pin.

#### Underwater Swimming

First Prize—Gold Watch Charm.

Second Prize—Bronze Watch Charm.


Friday, July 24, 1908. 2 P. M.

#### Base Ball—Detroit Yachtsmen vs Toledo Yachtsmen

Winners—1 fine box Key West Cigars.

Losers—1 box Stogies.

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**THE HOLLENDEN  
 CLEVELAND**

## IMPORTANT NOTICES

The Judges will be Com. A. R. Boswell, Toronto, Ontario, Com. Geo. W. Gardner, Cleveland and Com. Chas. Scates, Chicago.

The headquarters of the Secretary-Treasurer will be at Walter Ladd's boat landing, where all Captains should report after registering on the Flagship.

All articles lost or found should be reported to the Secretary-Treasurer.

The headquarters of all Committees will be at the office of the Secretary-Treasurer.

Yacht Captains will please watch for signals on the Flagship. Code will be found in manual.

Guns must not be fired from any yacht within thirty minutes of the time of starting a race, and not until every yacht has crossed the line.

Steamers and sailing yachts following the races must keep to leeward, and at such distance as not to interfere with the maneuvers of the competing yachts.

All announcements of races and other events will be posted on the bulletin board at the headquarters of the Secretary-Treasurer.

All yachtsmen who attend the Ball are requested to appear in yachting uniform, with coats. Tickets for the Ball may be secured from the Secretary-Treasurer or members of the Ball Committee.

One or more special cars will leave the Hotel Victory every morning half an hour before the firing of the preliminary gun. It is necessary to catch this car in order to see the start of the races. The dining-room at the Hotel Victory will be opened at 6:30 A. M., in order to allow time for breakfast.

There will be no entrance fees. The races are open to every yacht or boat which complies with the rules of the Regatta Committee, and which is regularly enrolled in any Club in the Association, or which has been invited to participate by the same.

The Revenue Cutter Morrill, as stated on page 9, has been detailed by the Treasury Department at Washington to patrol the course. She will be assisted by other boats appointed by her Captain. They will have the authority to enforce the regulations, and will display a blue flag with white letter P.

# SKIDOO!

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H. P. Bare Engine** **23**

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and Fresh Water Boat Fittings,  
\$43.75. Salt Water Boat Fittings, \$5 extra.

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engine of its size on earth. Simple, compact,  
strong, durable, reversible, economical and safe.  
Anyone can install and run it. Drives boat 5 to 8  
miles an hour. Sold from factory to user on 30 day's ap-  
proval and 6-year guarantee. Prompt shipment. Order now.

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A boat to *go*

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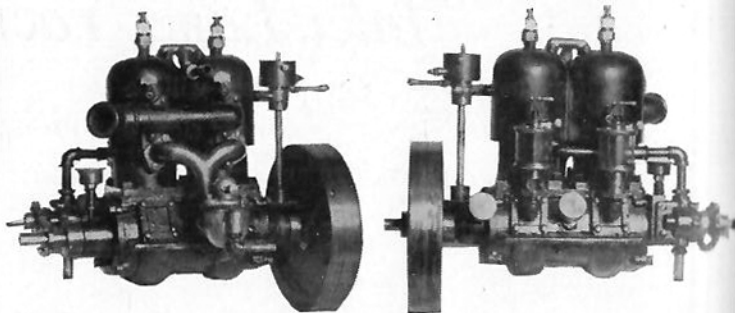
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Buffalo, N. Y.

*The History of the  
Inter-Lake Yachting Association*

*1884 - 1908*

*By*


*Robert E. Power*

*AUTHOR'S NOTE—No one who has not attempted to go back a few years into yachting history, can realize how incomplete are the early records and insufficient the data offered the writer for reference. With the administration changing from place to place each year, many valuable papers have been lost and those that exist with the present Secretary are in a chaotic state of confusion. I know that many important facts have been omitted in this story and that errors will probably be apparent to the well informed reader. It would appear to be an absolute necessity for the Association to catalog and file the records still remaining and either appoint a historian or furnish the Secretary with means to preserve documentary evidence of the future.*



GEORGE H. WORTHINGTON OF CLEVELAND  
Commodore of the Inter-Lake Yachting Association, 1896—1905—1908

# The Inter-Lake Yachting Association



TO WRITE the early history of the Inter-Lake Yachting Association requires considerable imagination. Like the immortal Topsy, it "just grewed." The Association was in existence, at least in purpose if not in fact, as early as 1884, although we have only casual evidence of its inception. The actual date of the official organization of the Inter-Lake Yachting Association, taken from the most authentic record we have, is January 17, 1885, and its birthplace, Cleveland, O.

Having identified our story with a definite beginning, we must drift back for a moment before getting under way in earnest. Prior to 1884 the active yacht clubs in the Lake Erie district were few and far between. Detroit had one and a very excellent organization, the

Michigan Y. C.; Toledo supported two clubs, the Toledo Y. C. and the Ohio Y. C.; the Cleveland Y. C. represented the Forest City, and at the far end of the lake was the Buffalo Y. C. All of these clubs gave open regattas in the early 80's, and the sport flourished with good fleets and fine sailors. Yachtsmen from one port found it no hardship to visit those far distant, and the racing and cruising was keen and lively. Boats were boats in those days, carrying a man-o'-war's crew with accommodations on board for all hands whether in port or outside. It was customary for a man to take his friends to and from the regattas and it was on one of these annual cruises that our yarn begins.

In the summer of 1884, Henry Gerlach, a veteran Cleveland yachtsman, invited a small party of friends to cruise up the lake on his sloop Lulu. Their destination was Toledo, where a regatta

was about to be held. In the party was Commodore Geo. W. Gardner, and it was upon his suggestion that the talk centered on the formation of an association of all yacht clubs in the Lake Erie district. The matter was further considered at Toledo and met with general favor among the yachtsmen present at the meet. So it came about that a joint meeting of the Cleveland Yachting Association and the Cleveland Canoe Club was called in Cleveland on January 17, 1885, and the Inter-Lake Yachting Association became a fact. Commodore Gardner was elected President and J. S. Williams, Secretary. It was arranged to hold a joint regatta at Put in Bay in the summer of 1885.

In that year a preliminary regatta of sailing yachts (for gasoline craft were hardly thought of in those days) was held on Lake St. Clair under the auspices of the Michigan Y. C. Seventy-two yachts took part and when the races were over threescore of these set sail for the Islands. That squadron run to Put in Bay in a northeast gale is still talked about by the old fellows, and the safe arrival of every boat speaks well for the staunch hulls and thorough seamanship of the men. Such beginning was indeed auspicious and gave the new project a healthy start. Commodore Gardner's flagship in that first regatta was the schooner Louise, owned by Vice-Commodore Hart, of Detroit, and these gentlemen carried through a most successful regatta.

Regattas were held in 1886 and 1887 and then dissension broke out at Toledo between the Ohio and Toledo Yacht Clubs. It is hardly necessary to go into the details of this affair; we simply mention it because it caused a postponement of the 1888 regatta and the I.-L. Y. A. had no gathering that year. On February 23, 1889, Commodore Gardner, still the presiding officer, called a meeting of yachtsmen at Detroit and it was decided to hold a regatta at Put in Bay in July, 1889. The regatta committee that year was

headed by Commodore Albertson, of the Michigan Y. C. "Joe" Hepburn, of the Toledo Y. C., was also an active member of the committee. Race meets were held in 1889, 1890, 1891 and 1892. During that period the Citizens' Y. C. of Detroit came into the ranks and also the Sandusky and Put in Bay Y. C. The 1892 meet was held at Cleveland.

The panic of 1893 practically killed the Inter-Lake Association, just as it did many another prosperous enterprise. To all intents and purposes, the Association as it existed during the period of ten years previous, may be said to have died a natural death. It had always been more or less intermittent in its effect upon the sport and exerted no decided influence along what were even then, modern lines of organization. We have no record of the regattas, no draft of the rules or the conditions under which the races were sailed, nor anything to show that the clubs in membership were united except during the time of the summer regattas.

A few bits of sunlight came down out of that hazy past. We find that the Canadians visited the upper lakes more often than in later years, and the names of Commodores Monck, of Hamilton, and Jarvis, of Toronto, are especially to be remembered. Both are still in the sport and will be with us this year. Captain John Prindeville, of Chicago, and Captain Joe Nicholson, of Detroit, were two veterans of the '80's who accomplished much for yachting on Lake Erie before they were summoned for their last long cruise.



HE year 1894 witnessed the reorganization of the Inter-Lake Yachting Association and the beginning of its present uninterrupted career. Commodore Geo. W. Gardner, of Cleveland, the founder of the Association ten years previous, and who, during that whole period, had been undoubtedly the most active patron of the sport on Lake Erie, was chosen to again take the executive office. He had been for many years Commodore of the Cleveland Y. C., as well as the original Inter-Lake Association, and was beloved and respected throughout the chain of fresh water lakes. The following men were elected to serve with him: Vice-Commodore, John E. Gunckel,



COMMODORE GEORGE W. GARDNER  
Founder of the Inter-Lake Yachting Association



O. Y. C.; Rear Commodore, H. C. Kendall, West End Y. C.; Secretary-Treasurer, E. W. Radder, Cleveland Y. C. Although the official records are not in existence, tradition says that the following clubs were charter members in 1894: Cleveland Y. C., Toledo Y. C., Ohio Y. C. of Toledo, Buffalo Y. C., Sandusky Y. C., Citizens' Y. Association of Detroit; West End Y. C. of Detroit, Put in Bay Y. C., Erie Y. C.

The first annual-regatta was held at Put in Bay August 8, 9 and 10, 1894. The big feature was the contest for the *World* cup, a \$200 trophy, put up by the Cleveland newspaper of that name. It was a handicap race for classes C, D and E, and had fifteen starters. The winner was Chinook, of the Sandusky Y. C.; Brownie, the wonderful little Toledo 18-footer, was second; Marie M, of Sandusky, third; Commodore Gardner, of Cleveland, fourth; Whim, of Cleveland, fifth; Huntress, of Detroit; Lotus, of Toledo; Grayling, of Cleveland; Henriette, of Detroit; Alert, of Cleveland, all well-known craft, finished in order.

Dinah, of Hamilton, Ont., won the race in class B, beating Sultana, of Toledo; City of the Straits, of Detroit; Surprise, of Detroit, and Scorpion, of Toledo. The Cleveland cutter Neva, owned by Percy W. Rice, was disabled. Class C race on August 10 went to Shamrock, of Detroit, the same black centerboard sloop that has been owned at Cleveland for so many years. She won \$40 first prize. Scud, Enright, Alborak, Lulu B and Grayling also started. The free-for-all race went to Cynthia, of Sandusky, together with \$50 in gold. Sultana, Enright, and Neva finished in order.

In that first year there was a race for naphtha launches, the entries being Restless and Sweetheart, both 6-horsepower outfits. Restless won on time allowance, being four feet shorter o. a. length.

As the measurement rules were succeeded the following year by the well known Seawanhaka formula, let us record the original set simply to show under what provisions the old boats were built and raced. The following extract is taken from a regatta announcement at Toledo in 1894:

Measurement—The gross measurement of boats shall be determined by adding together the length of the water-line, the great-

est breadth of beam and the depth of hold from the under side of the deck to the top side of the planking, at or as near the mast as possible.

Class A—All boats measuring 35 feet or over, on the water-line.

Class B—All boats measuring 30 feet or over, and under 35 feet on the water-line.

Class C—All boats measuring 25 feet or over, and under 30 feet on the water-line.

Class D—All boats measuring 20 feet or over, and under 25 feet on the water-line.

Class E—All boats measuring less than 20 feet on the water-line.

Time Allowance—Two seconds for each foot difference in actual gross measurement, will be allowed for each mile in the course. But in computing the time allowance for schooners four-fifths, and for yawls seven-eighths of the water-line shall be taken in place of full water-line.

Another feature since done away with, was the firing of a starting and a handicap gun with five minutes intervening. The boats were timed as they actually crossed the starting line any time during that period; if they failed to get away before the second gun their start was taken on the gun and they lost the time they were late in crossing. This custom held during 1895 and 1896, but was abolished by the 1897 administration.

The new boat of the year was the steel cutter, Sultana, built at Wyandot for Commodore E. D. Potter, Jr., of the Toledo Y. C. She was some boat in those days and is still in active service. For a dozen years she raced at Put in Bay and won more than her share of prizes. Her chief purpose in her maiden year was to defeat the crack Cleveland cutter Neva, which she did to the great joy of the Toledo contingent.



THE Seawanhaka rule  $\frac{L W L + \sqrt{S A}}{2}$  came into use

in 1895, having been rather hastily adopted at a summer meeting the year before. It meant an entirely new order of things and the reclassification of the boats caused considerable discussion in the clubs. The rule provided for

seven classes, 20, 25, 30, 35, 40, 46 and 55-foot class. All boats rating above and below the limits were in the 55 and 20-foot class respectively.

The 1895 regatta was held at Put in Bay the week of August 6th, under the direction of the following officers: Commodore John E. Gunckel, of the Ohio Y. C., Toledo; Vice-Commodore Harry C. Kendall, of the West End Y. C., Detroit, captain-owner of Shamrock; Rear-Commodore J. J. Gill, of S. Y. C.; Secretary-Treasurer Frank R. Frey, of the Toledo Y. C., who had successfully sailed the sloop Brownie in 1894, and was this year on board the new sloop Elfin. He had even then made a reputation as an expert skipper and a keen racing man in his club, and was Toledo's representative in the councils of the I-L. Y. A. That was fourteen years ago, and he is still on the job in the same quiet, efficient manner.

Arlington U. Betts, of the Ohio Y. C., of Detroit, was fleet captain, and an able assistant for Commodore Gunckel. He brought out a new 38-foot launch, built for him by the Craigs and equipped with a Daimler motor. It was considered a matter of great note in those days that she was equipped with a mechanism for reversing the propeller.

In 1894, as in previous years, all the yachts sailed a 21-mile triangle, which took the contestants far from the starting line and made it a dull affair for the spectators. The 1895 committee changed this practice and laid out a triangular course  $3\frac{1}{2}$  miles to the leg to be sailed twice around. The starting line lay between Middle Bass and Gibraltar with the first leg N. N. W., the second S. W.  $\frac{3}{4}$  S. and the leg home E.  $\frac{1}{2}$  S.; these courses are exactly the same today, although the boats now sail but once around a shorter triangle.

The race on Tuesday, August 6th, was for 25-footers, 30-footers and a special class of single and double cats and for'n'fters. All were started in the afternoon and the contestants got a regular island squall, which did no damage except to disable the naphtha launches which did not finish. Clytie, T. Y. C., won in the special class, beating Bonita, C. Y. C., and Alene, T. Y. C. Marie M., of Sandusky, made a runaway match of the 25-foot race with Elfin,



JOHN E. GUNCKEL OF TOLEDO  
Commodore of the Inter-Lake Yachting Association, 1895

T. Y. C., Brownie, T. Y. C., and Jester, S. Y. C., in order. In the 30-footers, Undine, a Put in Bay Y. C. double cat boat, beat the Commodore Gardner by a narrow margin and Mr. Radder protested her measurement. Undine was awarded the prize. Lucy, C. Y. C.; Wah-Wah-Tay-Sec, E. Y. C., and Sirocco, W. E. Y. C., were a half hour behind.



FRANK R. FREY  
Sec'y-Treas. I-L. Y. A. 1895-'96



O. K. SCHIMANSKY  
Sec'y-Treas. I-L. Y. A. 1897

There were two days of racing for the larger classes, 40, 46 and 55-footers. S. O. Richardson's new sloop, Puritana, of Toledo, had two easy wins and took the champion flag in the 40-foot class. Josephine, C. Y. C., was second each day. Scud, O. Y. C., and Albatross, E. Y. C., also ran. Sultana, of Toledo, made it two straight in the 46-foot class, beating City of the Straits and Sur-

prise, both of Detroit. Neva, of Cleveland, had only one competitor in the 55-foot class and took two races. Avocot, also of Cleveland, was second.

Comparing the above entries with the number of boats known to have been racing in the various clubs, it is evident that the new rules were not exactly popular. There was time allowance in each class and each race was practically a free-for-all within certain measurement limits.

Puritana was the notable new entry in 1895. She was launched that spring at Racine, Wis., for S. O. Richardson, Jr., of Toledo, and carried a sloop rig of generous proportions. Sultana had the crack crew with Rol Potter at the stick and "Kid" Wilds on the headsails. They made things hum all along the circuit that year.

The queen of the I-L. Y. A. fleet in 1895 was the schooner Priscilla, owned by Geo. H. Worthington and Dr. E. E. Beeman, of Cleveland. She did not race because there was nothing in her class. However, she had shown her mettle at Milwaukee, July 4th, by sailing the Lake Michigan fleet hull down and making a spectacle of the Chicago schooner Idler. This Lake Michigan visit is noted especially because it brought a return visit in 1896 on the part of the big Chicago boats, who came around to Lake Erie to honor the new Commodore and his handsome craft.

Out of the wreck of the Michigan Y. C. came a new member to the Association in 1896, the Detroit Y. C., making ten clubs in membership when the season opened.

### 1896

### OFFICERS

Commodore .....	GEORGE H. WORTHINGTON..	Cleveland Y. C.
Vice-Commodore .....	C. J. LICHTENBERG.....	Detroit Y. C.
Rear-Commodore .....	HENRY TRACY.....	Toledo Y. C.
Secretary-Treasurer ....	FRANK R. FREY.....	Toledo Y. C.



WE have just shown why Commodore Geo. H. Worthington, of the Cleveland Y. C., was the logical successor to the highest office in the Inter-Lake Association, and it is almost superfluous to say that he gave the sport on Lake Erie more prestige among yachtsmen on the neighboring

lakes than it ever enjoyed before or since that memorable year, 1896. One could write a book and not tell all that happened on Lake Erie, but we must hurry on.

The regatta was held at Put in Bay, August 18-21, and was unquestionably the best attended and most successful meet ever given by the Association. Vencedor, of Chicago, and Canada, of Toronto, were due to battle for the Canada's cup at Toledo the following week, and this stellar event brought a big attendant fleet from Lakes Michigan and Ontario. They stopped at Put in Bay and gave the annual regatta an international character; incidentally the Canadians took about all the money and left behind a lot of disappointed but ambitious yachtsmen.

On Tuesday, August 18th, four classes raced. Priscilla beat the Chicago schooner Mistral in the first class. In the 50-foot class, Vanenna, of Chicago, defeated Vreda, of Toronto. The 40-footer Vivian, of Toronto, beat her Ontario sister Dinah, while Eva, also a Canadian, took the Lake Erie 35-footer Shamrock into camp.

Wednesday, August 19th, witnessed four races for 46, 30 and 25-footers and naphthas. Canada, of Toronto, beat Zelma and Czarina in the big class as a tuning-up race for her later victory over Vencedor. Hiawatha, of Toronto, won the race for 30-footers with Myrna, also of Toronto, second and Nox, of Rochester, third. The yawl Lucy, of Detroit, finished second but was disqualified. Yankee, Mischief, Undine and Viking, all from Lake Erie, also ran. In the 25-foot class Typhoon, of Toledo, beat the schooner Marie with Brownie and Pearl, also of Toledo, in order. There were ten starters. Restless won the naphtha race.

Thursday, August 20th, brought on another new feature, the first annual free-for-all race for the Hotel Victory cup. It was won by the Canadian 46-footer, Zelma, with the 40-footer Vreda of Toronto, second. Sultana, Vanenna, Marian and Priscilla finished in order. The consolation race on Friday went to the Cleveland cutter Neva, with Yankee, Lucy, Elfin and Mischief dividing \$200 in prize money. Certainly some consolation for slow boats.

Shamrock hailed from Cleveland this year, being owned by Messrs. Barth, Luetkemeyer, Perkins and Hoffman. Under new management she did well when it is considered that she was six



S. O. RICHARDSON OF TOLEDO  
Commodore of the Inter-Lake Yachting Association, 1897

years old, having been built in 1890 by J. W. Hepburn, of Toledo, for a Mr. Mitchell, of that city; later she went to Detroit and then to Cleveland, where she is still in commission at Rocky River.

Typhoon, Viking, of Toledo, and Myrona, were new boats, and the first named was the only Lake Erie sloop to win a flag. Viking was sailed by Secretary Frey, who was making a reputation for bringing out a new boat every season. Typhoon was owned by S. Campau, of the Ohio Y. C. Priscilla, the Cleveland schooner, was the 1896 flagship.

As a result of this regatta and the international races, there was a great boom in yachting, especially at Toledo and Cleveland. Sandusky was beginning to look up with the result that Geo. F. Anderson later in the season visited Toronto and brought back the Canadian cutter Eva, to race under the S. Y. C. burgee. Captain Herman Krause, of Cleveland, built the sloop Vinco and so it went all along the line. Let us hope the visit of the Canadians this summer will do as much to revive the sport as did their triumphal trip in 1896.

Just before the year 1897 was ushered in, the Toledo and Ohio Yacht Clubs settled their differences and the two organizations were merged into one club called the Toledo Yachting Association. The first Commodore of the united club was S. O. Richardson, Jr. He served until 1899 when Frank Frey succeeded him.

## 1897

### OFFICERS

Commodore, S. O. Richardson, Jr. . . . . Toledo Y. C.  
 Vice-Commodore, Henry Look . . . . . Citizens' Y. C.  
 Rear-Commodore, George T. Bliss . . . . . Erie Y. C.  
 Secretary-Treasurer, O. K. Schimansky . . . . . Sandusky Y. C.  
 Fleet Captain, W. L. Otis . . . . . Cleveland Y. C.  
 Measurer, J. W. Hepburn . . . . . Toledo Y. C.  
 Chairman Regatta Committee, R. D. Potter . . . . . Toledo Y. C.



COMMODORE RICHARDSON was supremely the logical choice for the executive office in 1897. He had regenerated the sport in Toledo by joining the warring factions, and his ability as an organizer and a hustler

have been proven many, many times. Is it presuming too much to prophesy that history will repeat itself and that he will again succeed Commodore Worthington next December?

Another new member came to the Association in the winter of 1897, the Detroit Boat Club, bringing the original quota of ten clubs into membership. The annual regatta was held at Put in Bay, July 5-9, 1897.

The regatta committee had been instructed to spend not more than \$650 in prizes for seven classes and naphthas. Just to show how they did it in those early days let us jot down a list of cash prizes

Prizes for each race, two races for each class:

Class.	First.	Second.	Third.
First Class—Champ. Flag and . . . . .	\$25.00	\$10.00	\$ . . .
52-Foot Class—Champ. Flag and . . . . .	25.00	10.00	. . . . .
42-Foot Class—Champ. Flag and . . . . .	25.00	15.00	. . . . .
37-Foot Class—Champ. Flag and . . . . .	20.00	10.00	7.00
32-Foot Class—Champ. Flag and . . . . .	20.00	10.00	7.00
27-Foot Class—Champ. Flag and . . . . .	20.00	10.00	7.00
22-Foot Class—Champ. Flag and . . . . .	10.00	5.00	3.00
Special Class—Champ. Flag and . . . . .	10.00	5.00	3.00
Naphtha Race—Championship Flag . . . . .	. . . . .	. . . . .	. . . . .
Free-for-All—Hotel Victory Cup . . . . .	20.00	10.00	5.00

In that year the ball was held on Monday evening at Hotel Victory and the smoker at Beebe House Hall Wednesday night. The racing started Tuesday morning, July 6th, for 52, 42 and 37-foot classes over the 10½-mile triangular course, twice around. In the 52-foot class Vanenna, of Chicago, won. The 42-foot class was easy for Sultana, of Toledo, while the 37-foot class went to Shamrock, of Cleveland.

On Wednesday, the 32 and 37-footers raced over the same triangular course, once around. Eva, scratch boat, owned and sailed by Geo. F. Anderson, of Sandusky, beat the Test, also of Sandusky, with Viking under Frank Frey, third. Test should not have been in the class, measuring only 24.93. Myrna, scratch boat, owned by C. M. Eddy, of the Toledo Y. C., won the race for 27-footers. Sirocco, sailed by "Jack" Rathbone, of the Detroit B. C., was

second, with Vinco, of Cleveland, third, and Typhoon, of Toledo, fourth. All of these races were sailed with handicaps in the classes. Scarlet, of Cleveland, won the power boat race, making seven miles an hour.

The race on Thursday was windward and leeward for all classes. In the 42-foot class, Sultana won again, with Czarina second. Shamrock again beat Puritana in the 37-foot class, while in the 32-foot class Test finished ahead of Eva and Viking. Typhoon won in the 27-footers, beating Myrna and Vinco.

The Victory Cup free-for-all was sailed over a short course, three miles to leeward and return, starting from in front of the hotel on the west side of the Island. Typhoon won, Eva second and Vanenna, Shamrock and Test in order. Sultana was disqualified because professionals sailed her in the race. It was rather a slow, dull contest throughout. The racing sloop Puritana was Commodore Richardson's flagship in 1897.

Before passing on, let us note the fact that the I.-L. Y. A. was just beginning to feel the influence of the Yacht Racing Union, formed at Erie during the Canadian visit in 1896. Its object was to join the yachting interests and unify racing rules on the Great Lakes. This feeling of fellowship and consolidation was epidemic at that time, and there was held on May 1, 1897, at New York a meeting of representatives of 108 American yacht clubs to form a national association for the adoption of uniform racing rules. The Seawanhaka rule on the lakes was well liked by some, but a composite formula suiting all hands was the object sought. Later on this brought into use Hyslop's girth rule adopted November 26, 1898. In all these deliberations the I.-L. Y. A., represented by Messrs. Richardson and Radder, took an active part. The latter was one of those keen, active yachtsmen who accomplish so much for the sport and yet never stand forth in the limelight of be-starred publicity. Ernest W. Radder, though never a Commodore, ranked among the foremost men in the I.-L. Y. A. during those early years, and the high standard which was maintained in the sport on Lake Erie is a monument to his memory. His sudden death in 1903 brought to an end the career of a thorough sailor-man and a true gentleman of the older school.



ALEX I. McLEOD OF DETROIT  
Commodore of the Inter-Lake Yachting Association, 1898

OFFICERS

Commodore, Alex. I. McLeod.....Detroit Y. C.  
 Vice-Commodore, Henry Tracy ..... Toledo Y. A.  
 Rear-Commodore, George T. Bliss.....Erie Yacht Club  
 Secretary-Treasurer, Otto F. Barthel.....West End Y. C.  
 Fleet Captain, J. H. Beattie.....West End Y. C.  
 Measurer, J. W. Hepburn ..... Toledo Y. A.  
 Chairman Regatta Committee, George F. Anderson..Sandusky Y. C.

**T**HE 1898 administration was elected at Cleveland, February 12, 1898, and began its career with a balance of \$34.17 in the treasury. Certain slight changes in the rules were made, about the most important being an amendment stating that yachts of 42 feet length and under must be steered by a Corinthian, a member of one of the clubs in the Association, and must be manned by Corinthians, except that any such boat may carry the number of professionals regularly employed on the boat, not to exceed two in any case. The Citizens' Y. C., of Detroit, filed its resignation, which was accepted. The annual regatta was scheduled for Put in Bay, July 25-29.

As in the previous year, the race committee was handicapped by lack of funds, with the result that they did away with the first class and added a 22-foot and a half-rater class to the list. The cash prizes totaled \$406.50. Some revenue was derived from program advertising this year for the first time.

The races started on Tuesday morning, July 26th. Surprise, of Detroit, won in the 52-foot class. The 42-foot class race went to Josephine, while the 32-foot class was won by Viking, of Toledo. Otsiketa, of Detroit, won the naphtha race. The following day, Wednesday, Puritana, of Toledo, won in the 37-foot class, while Carrie B, of Detroit, beat Typhoon, of Toledo, and Wrinkle, of Sandusky, in the race for 27-footers. Mabel L, of Toledo, won in the 22-foot class.

The races Thursday morning were postponed until afternoon because of lack of wind. It was the proverbial calm before the

storm, because the boats were hit by one of the worst squalls in the history of yacht racing at Put in Bay. Mabel, of Toledo, and Carrie B, of Detroit, were capsized and Wrinkle was driven ashore on Middle Bass Island. Surprise, of Detroit, won in the 52-foot class; Alice Enright, of Toledo, in the 42-foot class; Puritana, of Toledo, in the 37-foot class, and Vinco, of Cleveland, in the 27-foot



E. P. SHARPE  
 Rear Com. I.L. Y. A. 1900



OTTO F. BARTHEL  
 Sec'y-Treas. I.L. Y. A. 1898

class. It was a day which the old sailors remember well.

The free-for-all for the Victory cup on Friday was won by Puritana, of Toledo, with Surprise, of Detroit, second, and Eva, of Sandusky, third. The race for half-raters was won by Vivian, built, owned and sailed by Joe Grasser, of Toledo. Our genial Secretary at that time was a youngster in knickerbockers, and he

began his Inter-Lake career in the most auspicious manner. Commodore McLeod's steam yacht Louise was the flagship of the fleet.

In spite of the fact that the Spanish-American war had taken many of the yachtsmen to the front, the annual regatta was successful, and the year one of the most prosperous the Association had had up to this time. Secretary Barthel turned over a balance of \$58.57 to the incoming administration.

### 1899 OFFICERS

Commodore, George T. Bliss.....Erie Y. C.  
 Vice-Commodore, George F. Anderson...Sandusky Y. C.  
 Rear-Commodore, Otto F. Bartnel.....West End Y. C.  
 Secretary-Treasurer, L. D. Davis.....Erie Y. C.  
 Fleet Captain, O. J. Mulford.....Detroit Boat Club  
 Measurer, J. W. Hepburn.....Toledo Y. A.  
 Chairman Regatta Committee, W. R. Huntington...Sandusky Y. C.



HE record of 1899 really begins on November 26, 1898, when a special meeting was called at Toledo to consider proposed amendments to the racing rules. These came about as has been noted before, through the Association's representation in the Y. R. U., of North America. The amendments constituted the necessary change to the girth rule, the formula for which was as follows:

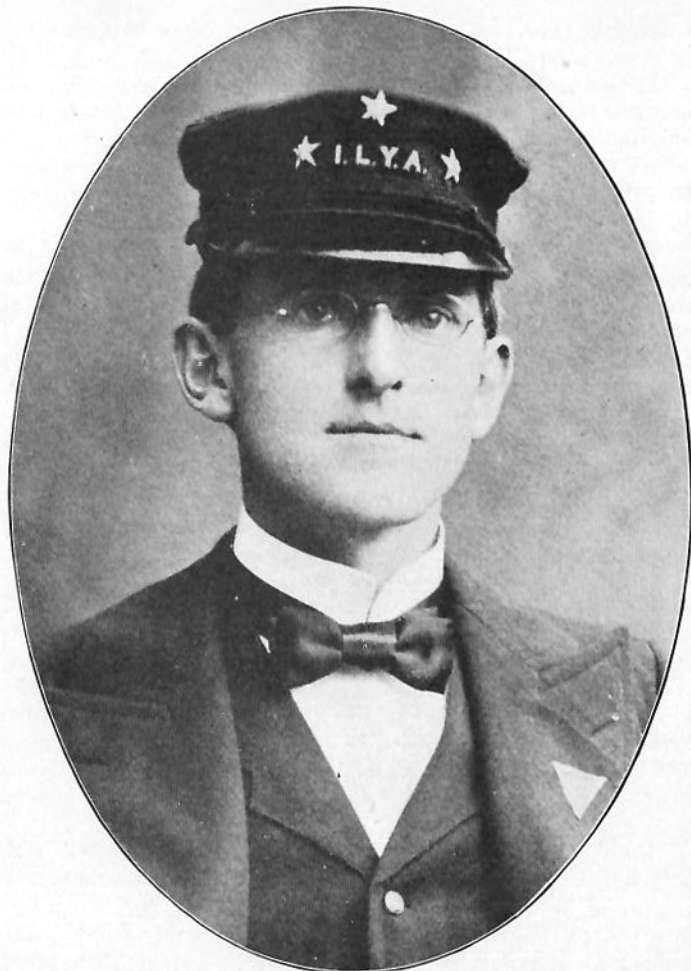
$$R. L. = \frac{L. W. L. + B + 75\% G + 50\% \sqrt{S. A.}}{2}$$

B—Equals beam taken from outside to outside of planking at the broadest part of the yacht.

G—Equals girth taken from water-line to water-line under the keel at a point 0.6 of the distance between the outer edge of the L. W. L. marks from the fore end.

It meant a measurement of the immersed cross-section with all hollows filled in. Centerboards were taxed and when ballasted were measured as fixed keels. A new classification was adopted as follows:

First class (over 45-foot R. L.), 45-ft., 40-ft., 35-ft., 30-ft., 25-ft. and 20-ft. In class racing, time allowance, with the exception



GEORGE T. BLISS OF ERIE  
 Commodore of the Inter-Lake Yachting Association, 1899



of the first class, was done away with and every boat was expected to assume the maximum limit of her class.

The delegates fought the last amendment and succeeded in passing a rule permitting existing yachts to remain in their classes, provided the new measurement carried them over the limit.

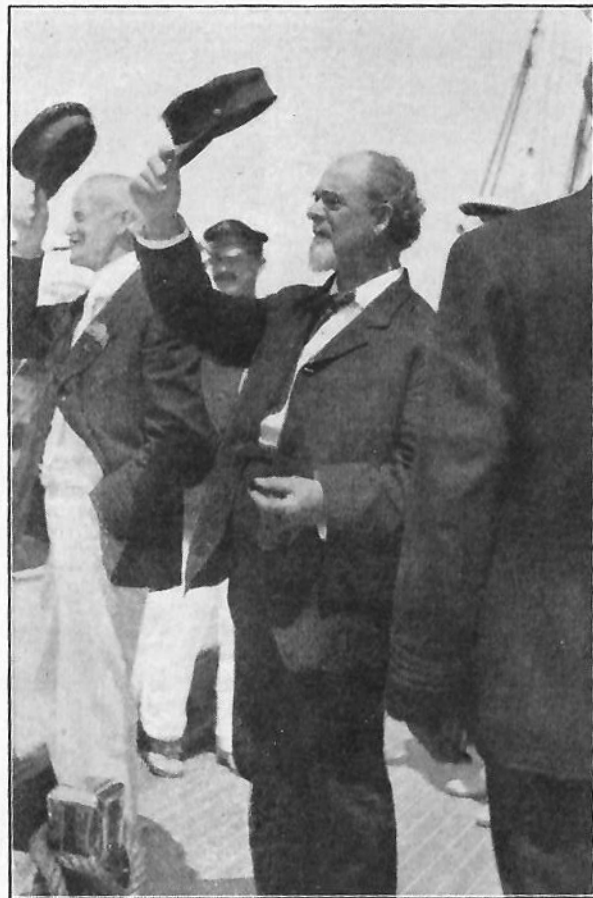
We have dwelt upon the record of this meeting to considerable extent, because it was a big step in the evolution of yacht racing on Lake Erie.

The annual meeting and election of officers was held at the Russell House, Detroit, February 11, 1899. The Detroit Boat Club resigned and the Detroit Boat Club Yachtsmen was substituted in its place. The Up-River Yacht Club, of Toledo, was also elected to membership. Thus the Association had ten clubs on its roll in 1899. The date of the annual meeting of the Association was changed from the second Saturday in February to the first Saturday in December of each year. The annual regatta was scheduled for Put in Bay, August 7-11.

In making up the race program, the regatta committee was confronted with lack of funds and this matter was the cause of several suggestions for increasing the revenue of the Association. Secretary Barthel suggested in his report at the annual meeting that the dues be placed on a per capita basis. The 1898 race committee in its report made a strong recommendation that the revenue of the Association be increased and that more money be allowed for prizes. At a special meeting held at Cleveland, May 20, 1899, it was decided to raise \$300 for the regatta committee by special contributions from the various clubs. The amounts ranged from \$100 from the Cleveland Y. C. to \$15 from Buffalo. A formal invitation was extended to all clubs in the Y. R. U. to take part in the annual meet.

The Put in Bay regatta opened Wednesday August 9th, with races for all classes, once around a 12-mile triangular course.

City of the Straits beat her Detroit sister Surprise in the first class. The 40-foot race was won by Viking, of Toledo, with Miriam, the Morrison sloop from Erie, a poor second. Eva, sailed by Vice-Commodore Anderson, of Sandusky, had an easy win over Carrie B, of Detroit, in the 35-foot class. The yawl Francis A, of



COMMODORE GARDNER

On board Priscilla, at Put in Bay, 1905

Detroit, owned by Alex. I. McLeod, won the 30-foot race with ease from Mabel R, of Toledo; Dominique, of Detroit, was third. Wrinkle, sailed by Emil Schmidt, of Sandusky, had an easy win over Golden Rod, of Toledo, in the 25-foot class; Leona, of Toledo, was third. The 20-foot race went to Sun Dancer, owned by Stephen Kinder, of the Up-River Y. C.; Kayak, of Sandusky, sailed by Frank Rawson, was second.

The free-for-all race for the Victory cup was sailed on Thursday, and the honor went to Wrinkle in a small field of eight boats. Wrinkle was beaten by every boat in the fleet but won on time allowance. Francis A, the Detroit 30-footer, was second; Eva, Sandusky 35-footer, third, and Juanita, Joe Taylor's 40-footer, fourth. It was a miserably slow race, taking 5¾ hours for about all of the boats. Restless, owned by W. C. Sterling, of the Toledo Y. C., won naphtha race.

On Friday the entry list dwindled to sixteen boats all told and ten of them divided \$222.50. It was certainly easy money. City of the Straits won again in the first class; Juanita, in the 40-foot class; Eva, in the 35-foot class; Francis A, in the 30-foot class, with Dominique second. Wrinkle practically took a sail over in the 25-foot class, while Kayak gave Sun Dancer an awful beating in the 20-foot class. Nadjy, of Sandusky, beat Nymph, of Toledo, in the half-rater division. The Toledo midget was sailed by Fred Pritchard, who later built Myth to defeat the Sandusky boat. It was another slow race, taking the boats nearly five hours to sail ten miles.

Despite the fact that \$515 in cash was distributed in the three races, the entry list was disappointing and the weather more so. The well-laid plans to secure the attendance of the Chicago fleet on its way to Lake Ontario for the Canada's cup races did not mature, nor did the I-L. Y. A. fleet make the cruise as a body down the lake to Erie and Port Colborne. Commodore Bliss's flagship in 1899 was the good ship Scorpion.

**1900**

**OFFICERS**

Commodore, George F. Anderson.....Sandusky Y. C.



**GEORGE F. ANDERSON OF SANDUSKY**  
Commodore of the Inter-Lake Yachting Association, 1900

Vice-Commodore, Otto F. Barthel. ....West End Y. C.  
 Rear-Commodore, E. P. Sharpe.....Buffalo Y. C.  
 Secretary-Treasurer, W. F. Seitz, Jr.....Sandusky Y. C.  
 Fleet Captain, Frank B. Rawson.....Sandusky Y. C.  
 Measurer, J. W. Hepburn.....Toledo, Y. C.  
 Chairman Regatta Committee, F. R. Frey.....Toledo Y. C.



W. F. SEITZ Jr.  
 Sec'y-Treas. I-L. Y. A. 1900-'01-'02

FRANK B. RAWSON  
 Vice-Com. I-L. Y. A. 1901

**T**HE annual meeting was held at the Reed House, Erie, December 2, 1899, only five visiting delegates being present. The treasurer's report was the bright particular feature, showing a balance of \$192.17 for the new administration. This was due partly because Put in Bay in those days gave \$235 to support the Association.

Commodore Anderson called his organization meeting at Sandusky, February 10, 1900. At this meeting the first official order for a squadron sail as a fixture at the annual regatta was issued. It was decided not to give up the girth rule as suggested by that versatile body, the Y. R. U. of North America. Put in Bay was selected for the annual regatta and July 23-27 named as the dates.

The racing in the annual regatta began Tuesday, July 24, for all sailing classes over the 12-mile triangular course. Viking won in the 40-foot class, beating Josephine and Juanita, of Toledo. Eva scored an easy win in the 35-foot class, while Dominique beat Tattoo, owned by C. B. Lockwood, of Sandusky, in the 30-foot class. As we recall this was Tattoo's maiden year and marked the beginning of her long and honorable career in the I-L. Y. A. Wrinkle, of Sandusky, won in the 25-foot class, while Myth took first among the 20-footers. There were in all sixteen starters. Eva, as usual, made the best time over the course, 2:18:32.

The free-for-all on Wednesday for the Victory cup was a corking good race between Eva, Juanita and Tattoo, the Sandusky 35-footer beating Joe Taylor and Kid Wilds by one minute, corrected time. Tattoo was thirteen seconds behind Juanita. Wrinkle was fourth and Sultana, back again after two years' absence and now owned by "Rube" Moore, of Detroit, was fifth, having made the fastest time over the course. City of the Straits and Viking also ran.

The windward and leeward race on Friday brought out 21 boats. Sultana beat City of the Straits and Enright in the 45-foot class. Viking beat Juanita in the 40-foot class, making the fastest time of the day. Eva took all the money in the 35-foot class as the judges never considered a second in that class. Tattoo won in the 30-foot class with Cricket, of Detroit, second, and Dominique third. Wrinkle repeated her victory in the 25-foot class, distancing Gracie and Privateer, her Toledo competitors. The 20-foot class race went to Alice, owned by J. E. Soules, of the Up-River Y. C., while Myth, also of Toledo, gave Nadja a bad beating in the 16-foot class. Dearest won the power boat race.

The entertainment features of the 1900 meet were in charge of John L. Dexter and were, according to his own report and "in

spite of his natural modesty," an unqualified success. Besides the squadron sail on Thursday, a chowder party at Wehrle's was a prime attraction. The racing cutter Eva was Commodore Anderson's flagship in 1900.

1901

## OFFICERS

Commodore, W. R. Huntington.....Cleveland and Sandusky Y. C.  
 Vice-Commodore, Frank B. Rawson.....Sandusky Y. C.  
 Rear-Commodore, J. N. Richardson.....Lakewood Y. C.  
 Secretary-Treasurer, W. F. Seitz, Jr.....Sandusky Y. C.  
 Fleet Captain, E. L. Marsh.....Sandusky Y. C.  
 Measurer, J. W. Hepburn.....Toledo Y. C.  
 Chairman-Regatta Committee, George F. Anderson.. Sandusky Y. C.



HE annual meeting was held at Sandusky, December 1, 1900. The Secretary-Treasurer's report showed a balance of \$113.01 on hand. Commodore J. C. Shaw, of the Detroit B. C. Y., was elected Commodore of the I-L. Y. A., but his resignation was accepted immediately. The Lakewood Y. C. was elected to membership. Discussion centered about the place for the annual regatta, it being finally voted to hold it at Detroit in 1901. This was later reconsidered and left open. The meeting finally adjourned subject to the call of the executive officer. Those were indeed stormy days.

Commodore Huntington called the adjourned meeting at Toledo, December 22, 1900. The Maumee River Y. C. applied for membership and was admitted, while the West End Y. C., of Detroit, resigned. The old fight against Put in Bay was renewed, but some concessions from the Hotel Victory prevailed and the old meeting place was again chosen. July 15-19, 1901, were the dates selected for the regatta.

The racing began on Tuesday, July 16, a race for all classes over a triangular course. The race was started after 11 o'clock and was a drifting match all afternoon. City of the Straits beat Sultana in the 45-foot class, and the old Shamrock, of the Lakewood Y. C., beat Viking and Juanita in the 40-foot class. Eva had



W. R. HUNTINGTON OF SANDUSKY  
 Commodore of the Inter-Lake Yachting Association, 1901

a sail-over against Meteor, of Lakewood, in the 35-foot class. Minerva, the new Detroit yawl owned by Thomas Gannon, of the D. B. Y. C., beat the Sandusky yawl, Lucinda, in the 30-foot class, while Tattoo, Dreamer and Typhoon were not placed. Wrinkle came back for her usual first in the 25-foot class, her only competitor being Plover, a new Lakewood craft owned by Henry Whiton. The 20-foot class went to another new Detroit yawl, Myrta, a Toledo craft, Voletta, being her only rival. Myth had a sail over in the 16-foot class.

The Victory cup on Wednesday was won by Juanita by a very narrow margin from the double-hull flyer Lucinda; only 17 seconds was the difference in corrected time. Eva was third, 31 seconds behind Lucinda, and Tattoo fourth; Wrinkle and Plover followed. Delta, of Toledo, won the power boat race.

The windward and leeward race on Friday, July 19, was the best race of the week as far as weather was concerned. City of the Straits won again in the 45's, while Viking took the race in the 40-foot class. Eva took a sail-over in the 35-foot class. Minerva repeated in the 30-foot class with Tattoo second, Lucinda failing to start. Wrinkle won again in the 25-foot class with Ripple, of Toledo, second. The 20-foot flag went to Myrta, of Detroit, and the 16-foot to Myth, of Toledo. There were 19 starters in all. Commodore Hutington's Seabury yacht, Thelma, was the flagship in 1901.

Chairman Dexter, of the entertainment committee, had his troubles at this meet. The ball on Tuesday evening was a slim affair, while the squadron sail, starting late on Thursday, caused the Wehrle chowder party to go by default.

We begin to see the passing of the old deep draft cutters. Such boats as Minerva and Lucinda were examples of what the designers were soon going to do to the rules. Considering that Minerva measured 29.9 and Lucinda 25.58, the Sandusky yawl was probably the fastest boat of her size, racing that summer at Put in Bay. She was designed by Crowninshield, measuring 35 feet over all, 21 feet water line, 8 feet 6 inches beam, and 3 feet 6 inches draft. Her sail area was 730 square feet. The Nilson Y. B. Co. launched her at Baltimore in 1901 for J. F. Donahue, of Sandusky.

Eva was still supreme, but the coming of the Canada's cup 35-footers, Cadillac and Detroit, soon spelled her doom on the race course. The Sandusky 35-footer had made a wonderful record, however. In six successive seasons she had been invincible, and no one tried to beat her the last two years. She was excellently handled by Commodore Anderson, and her crew is still referred to as one of the best that ever manned a fresh water racing craft.

The writer finds it necessary to digress for the moment into the records of the Y. R. U., because that body was in its prime at this period and was making history that is woven closely into the story of the Inter-Lake Yachting Association. The year 1901 brought on some unusual activity because of the Canada's cup trials at Chicago. Illinois, Milwaukee, Detroit and Cadillac were the most notable 35-footers built as defenders, while Canada put up Invader. In order to protect the rules, the Y. R. U. appointed a construction committee, consisting of Messrs. Crawford, Radder and Jarvis, and they had their hands full deciding whether tamarack could be used for oak, and similar irregularities. About every section in the book of rules was altered in some manner and fortified against the schemes of yacht designers.

In this year the Union threshed out the status of professionals in sustaining a protest against Arthur Pettie, who claimed the right to sail the Cadillac as an amateur. Another resolution which should be recorded stated that the "Union will recognize no races sailed on Sunday, and we request the clubs belonging thereto to urge upon their members the advisability of not permitting their yachts to enter into such races."

## 1902

### OFFICERS

Commodore, C. C. Warren.....	Sandusky Y. C.
Vice-Commodore, J. N. Richardson.....	Lakewood Y. C.
Rear-Commodore, F. R. Frey.....	Toledo Y. A.
Secretary-Treasurer, W. F. Seitz, Jr.....	Sandusky Y. C.
Fleet Captain	
Measurer, F. G. Kimball.....	U. R. Y. C.
Chairman Regatta Committee.	



C. C. WARREN OF SANDUSKY  
Commodore of the Inter-Lake Yachting Association, 1902



HE annual meeting of the I.-L. Y. A. was held at the West House, Sandusky, December 7, 1901. The Secretary-Treasurer's report showed a balance of \$27.23 on hand. Commodore Huntington presented a silver cup to the Association to be raced for at the annual regatta.

The 1902 meet was held at Put in Bay, July 21-26, the first race being called Tuesday morning, the 22nd. The course was triangular, 12 miles. Sultana defeated Oberon and Enright in the 45-foot class by a margin of 12 minutes. The 40-foot class was all Viking by 20 minutes over Juanita. Detroit made her I.-L. Y. A. debut in the 35-foot class, beating the old champion Eva 13 minutes. Cadillac was disqualified.

In the 30-foot class the new Detroit yawl Gloriana beat Tattoo and Lucinda, of Sandusky, while Minerva, which was owned by Geo. Smedley, of Detroit, failed to get a place. There were no 25-footers and only one 20-footer, Shome, of Detroit. The 16-foot race went to Myth, with Golden Rod, a Toledo cat, second.

On Wednesday, July 23, the windward and leeward race was sailed under light conditions. Sultana won again in the 45-foot class and Frank Frey in Viking repeated his victory in the 40's. Cadillac gave Detroit a two-minute beating in the 35-foot class with Eva half an hour behind. Minerva beat her 30-foot sister Lucinda 15 seconds with Gloriana 10 minutes behind the leader. Louise, of Toledo, sailed by Walter Coakley, won the 25-foot race without competition. Shome had another sail-over in the 20-foot class. Myth won in the 16-foot class.

The Victory cup free-for-all, sailed on Friday, brought the Detroit 35-footers into disgrace and gave a heat to that excellent little sloop Tattoo, of Sandusky. Both Detroit and Cadillac were disqualified for allowing a professional to steer them. Minerva was second, Lucinda third and Eva fourth. Gloriana got a beautiful beating.

John A. Rathbone won the power boat race with his launch Something. Commodore Warren's new Gloucester-built yawl, Wemco II., was the 1902 flagship.

Before the smoke of the 1902 season had disappeared, the Yacht Racing Union set a movement on foot to do away with the existing

girth rule and promote in its place five restricted classes—45, 40, 35, 30 and 25-ft. A table of restrictions and scantlings was adopted by the Union providing for the following measurement limits: Maximum L. W. L., maximum draft, maximum sail area, minimum W. L. beam, minimum area immersed cross-section, freeboard, etc.

For instance a 25-footer could be 25-ft. water-line, six feet draft

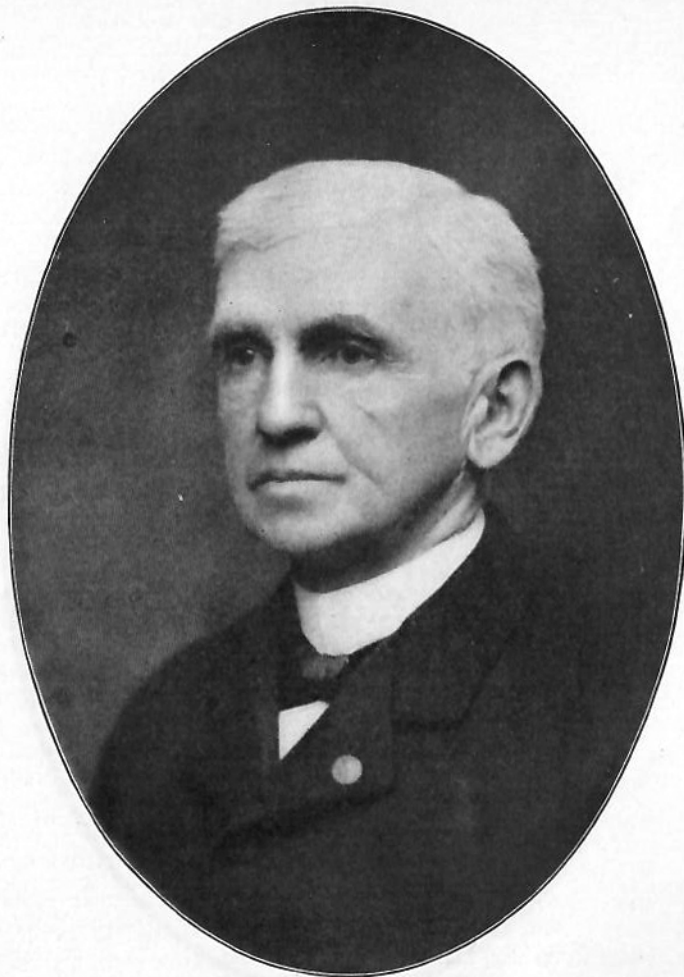


J. N. RICHARDSON  
Vice-Com. I-L. Y. A. 1902



W. C. STERLING  
Vice-Com. I-L. Y. A. 1904

and carry 1050 sq. ft. of sail. She must have 8 feet 3 inches water-line beam and 15.75 sq. ft. in her cross-section. The scantling table provided for certain sizes of frames, planking, etc. It was an elaborate schedule designed to compel the building of strong, able boats. Existing yachts were allowed to drop into the next class lower than the one they raced in under the girth rule.



HENRY TRACY OF TOLEDO  
Commodore of the Inter-Lake Yachting Association, 1903

We cannot begin to take space to describe these new rules in full, but the above gives a sufficient outline to show the evolution. The Inter-Lake adopted the Y. R. U. rules January 17 at Toledo and a new order of things was instituted in 1903. How the rule worked out will be apparent from comparing the 1902 and 1903 records.

During the winter the Monroe Y. C. joined the Association, while the Up-River Y. C. of Toledo resigned, having been merged in the Maumee River Y. C.

### 1903

### OFFICERS

Commodore, Henry Tracy.....	Toledo Y. A.
Vice-Commodore, J. H. Smedley.....	D. B. C. Y.
Rear-Commodore, W. C. Sterling.....	M. Y. C.
Secretary-Treasurer, A. N. Merrill.....	Toledo Y. A.
Fleet Captain, E. T. Affleck, Jr.....	Toledo Y. A.
Measurer, F. G. Kimball.....	M. R. Y. C.
Chairman Regatta Committe, J. W. Hepburn.....	Toledo Y. A.

**H**AVING adopted the Y. R. U. rules for all classes above 25 feet, it was necessary for the I.-L. Y. A. to add restrictions of its own for the small boats. These were grouped into three classes, 21, 18 and 16-foot, under provisions which were worked out but slightly and only called for practically a sail area and water-line measurement. The judges were lenient and not very thoroughly imbued with the new idea, so that the small boats entered and sailed about as they pleased.

The annual regatta was held at Put in Bay, July 20-25, 1903, with Commodore Tracy's yawl Dolomite, as the flagship. A fierce easterly storm delayed the Commodore's arrival, and the exposure of that trip is said to have hastened his death a few months later.

Racing began Tuesday morning over the triangular course. Oberon, of Toledo, beat Ethel in the 40-foot class, while Sultana, racing now as a 35-footer, outsailed Enright. In the 30-foot class were the Cadillac, Detroit and Eva and they finished in that order. The 25-foot race was a cinch for Chloris, a new entry from Lake-

wood. She was a product of the Massachusetts Bay 21-foot class, from Small Bros.' design and a fin keel scow of pronounced type. Sailed by her owners, Messrs. Phelps and Primett, she was invincible in her class and a fit companion for the Detroit 30's.

Tattoo beat Wrinkle, owned by Fred Pritchard, of Toledo, in the 21-foot class, while Louise beat Mermaid, a new Detroit center-



A. N. MERRILL  
Sec'y-Treas. I-L. Y. A. 1903

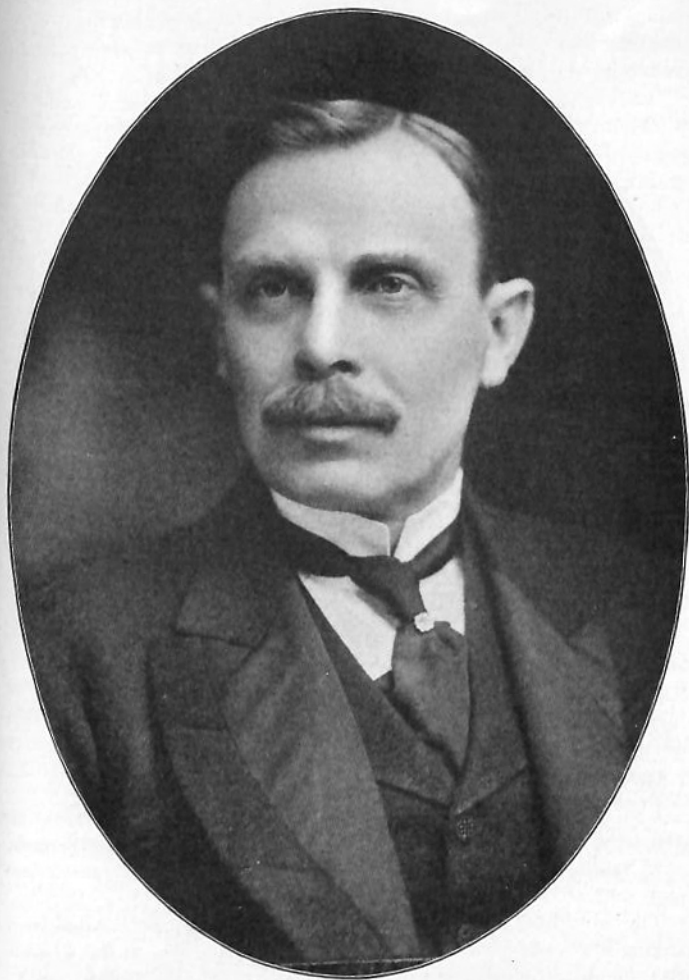


G. W. MAYTHAM  
Rear-Com. I-L. Y. A. 1904

board scow in the 18-foot class. Voletta, of Toledo, defeated Snark, a new Sandusky 16-footer, one of a number built that season to one design and raced with great spirit on Sandusky Bay.

The windward and leeward race on Wednesday was sailed in a spanking nor'wester. Merle trimmed the Toledo crowd in the 40-foot class; she was owned by Commodore Maytham, of Buffalo. Sul-





JOHN H. SMEDLEY OF DETROIT  
Commodore of the Inter-Lake Yachting Association, 1894

tana and Cadillac both repeated in the 35 and 30-foot classes, while Chloris sailed the Detroit yawl White Cap out of sight for the 25-foot flag. In the 21, 18 and 16-foot classes Tattoo again beat Wrinkle, Myrta beat Mermaid and Voletta repeated her victory of the first day. The power boat race was won by the naphtha launch La Voyageur.

A special race was sailed on Thursday between the large cruisers, Wemco II., Minx and Vesta, for the Commodore's cup. It was won easily by Commodore Warren's handsome yawl. Wemco is now owned in Chicago and has been renamed Juanita.

Friday morning was set for the Victory cup free-for-all, and it was conceded that no one had a chance but Cadillac, Detroit and Chloris. The small boats were practically barred, as an entry must take a measurement of at least 25 feet. The weather was light and Detroit captured the contest by a narrow margin from Chloris. Cadillac was third.

Shortly after the Inter-Lake regatta the 21-foot cabin class, instituted at Chicago in 1902 through the generosity of Sir Thomas J. Lipton, came to the notice of Lake Erie yachtsmen. The Country Club on Lake St. Clair began its yachting career by accepting from its president, Franklin H. Walker, Esq., a handsome trophy with conditions similar to those governing the Lipton cup contests. The first regatta was held in September, 1903, and the cup was won by Little Shamrock, a Chicago 21-footer. Her speed and the ease with which she outsailed the Detroit defenders, Taifu and Finesse, aroused the greatest interest; her advent was the first chapter in a new epoch of I.-L. Y. A. history. We shall hear much of the 21-footers before our yarn is spun.

### 1904 OFFICERS

Commodore, John H. Smedley.....	D. B. C. Y.
Vice-Commodore, W. C. Sterling.....	M. Y. C.
Rear-Commodore, G. W. Maytham.....	B. Y. C.
Secretary-Treasurer, H. C. Glidden.....	D. B. C. Y.
Fleet Captain, Northam Warren.....	D. B. Y. C.
Measurer, C. P. Sieder.....	D. Y. C.
Chairman Regatta Committee, John A. Rathbone.....	D. B. Y. C.



NINETEEN HUNDRED AND FOUR was the year de luxe in the Inter-Lake Yachting Association. It was featured by two things especially—the building of a new fleet of 21-footers and the abolishment of cash prizes in favor of an array of metal medallions and other luxurious appendages that plunged the Association from the heights of seeming prosperity to the uncertain depths of debt.

The annual meeting was held at Toledo, Dec. 5, 1903, in the new home of the T. Y. A. The important business of the meeting was the discussion about the new 21-foot class. Dr. Jennings in favor and Jno. A. Rathbone against it, fought the matter out for two days, when the affirmative prevailed and a resolution to adopt the class was filed. The Inter-Lake Cruising Club elected C. C. Warren of Sandusky its Commodore for the year 1904.

Shortly after the annual meeting Ex-Commodore Tracy died and his loss was keenly felt throughout the I-L. Y. A. circuit. He had been one of Toledo's standbys for many years and was one of the old school of yachtsmen that are fast passing away. In January, 1904, Commodore Affleck of the T. Y. A. bought the 70-foot yawl Hussar II from a Philadelphia yachtsman, which added another handsome craft to the Lake Erie fleet.

The organization meeting at Detroit February 20, ushering in Commodore Smedley's administration, was a magnificent social affair. The banquet room of the Russell House was decorated to represent a full rigged ship and every detail was carried out minutely. In the splendor of this gathering the business of the meeting was almost forgotten. About the most important action was the final adoption of the 21-foot cabin class. Commodore McLeod brought up the subject of Corinthianism and worked hard for a reinstatement of the Detroit professionals, Pettie and Senter. A provision for reinstatement of reformed professionals was adopted but the hole wasn't big enough to admit the aforementioned gentlemen. A special committee on racing rules and restrictions was favorably advocated by Dr. Jennings to become a fixture of the I-L. Y. A.

The annual regatta was set for July 17-23, 1904, at Put in Bay.

Sec'y-Treas. Glidden reported a balance of \$144.00 in the treasury. The Toledo Y. C. was added to the membership list in place of the Toledo Y. Ass'n, through change in name.

Before the 1904 season opened five new Detroit 21-footers were building, Ste. Claire designed by Crane for Franklin H. Walker, Pirate designed by Poulliot for E. L. Ford, Eyota designed by Burgess & Packard for Kenneth Stevenson, Ventura designed by Seabury for Jno. H. Smedley, and Spray designed by Small Bros. for H. P. Schmidt. W. F. Nash of the Lakewood Y. C. bought the Massachusetts Bay 21-footer Rooster II, which assured a fine fleet of fast boats. The 30-footer Detroit was sold to the Pacific Coast but Echota, the old Canada's cup boat, came into the I-L. Y. A. fleet through C. W. Schmidt of Cleveland, assuring Cadillac and her new owners, H. C. Kendall, et al. some competition.

The racing began at Put in Bay July 19 over the regular 12-mile triangle. Twenty-six boats started, comprising one of the finest I-L. Y. A. fleets ever gathered under the lee of Gibraltar Island. The weather was clear with a fair sailing breeze from the westward.

In the 40-foot class Czarina of Toledo beat Oberon 15 minutes. Enright of Toledo took the 35-foot race from Sultana and Juanita, while Cadillac walloped Echota for the 30-foot medal. Chloris had a sail-over in the 25-foot class as a matter of course.

The 21-foot restricted class was the star attraction. Spray had the legs and beat Ste. Claire almost two minutes, Rooster, Ventura, Pirate and Eyota finished in order. Tattoo won in the old 21-foot class with Finesse of Detroit second, Typhoon third and Mabel Mac fourth. Mermaid beat Wrinkle in the 18-foot race but was remeasured on protest and thrown out; the race went to Wrinkle. Myth, the old Toledo champion, owned by B. F. Davies of the Lakewood Y. C., won easily over Daphne, Truant and Wa Wa in the 16-foot class.

The windward and leeward race on Wednesday was sailed in a light westerly wind. Czarina and Enright repeated their victories in the 40 and 35-foot classes. Cadillac took a sail-over after Echota had withdrawn in the 30-foot class. Tattoo won again in the old 21-foot class with Mermaid second and Finesse third.

The 21-foot restricted class race was another fine contest between Spray and Ste. Claire, the former winning by 1 minute 37 seconds. Eyota was third with Pirate, Rooster and Ventura in order. Wrinkle won easily in the 18-foot class. Myth, Truant, and Daphne sailed practically a dead heat in the 16-foot class, the former being awarded the race after Daphne was measured out on protest.



W. J. BRODIE  
Sec'y-Treas. I.-L. Y. A. 1905



FRANKLIN H. WALKER  
Vice-Com. I.-L. Y. A. 1905

The Victory cup free for all brought on a fine legal scrap on the dock which lasted two days. "Jack" Rathbone with his knife well whetted and the authority of race chairman behind it, slashed the feelings of the 21-foot owners into shreds by refusing to allow them to compete for the old mug. He contended that they were not

built under Y. R. U. restrictions and were not eligible to race against sensible boats. He won out and the I.-L. Y. A. had to put up a special cup to pacify the rioters. It was indeed a joke on the chairman that the 25-footer Chloris, herself a Massachusetts restricted 21-footer, no better in constructive features and design than the I.-L. Y. A. class, should win the heat on the cup.

The race was started in a fair breeze which lightened, leaving the boats almost becalmed at the second stake of the triangular course. A squall made up from the southeast and the finish was a corker for all hands. Chloris and Ste. Claire passed Spray in the last quarter mile as the centerboard craft would not stand up in the hard wind. Cadillac was caught to leeward of the finish line and finished fourth. Eyota, Ventura, Tattoo, Rooster, Enright, Pirate and Wrinkle finished in order. This gave Wrinkle second place in the free for all with Tattoo third ahead of Cadillac on corrected time. Mabel Mac won the Huntington cup for old boats.

Two classes of power boats raced on Wednesday under the supervision of Prof. H. C. Sadler. The Kitchell won in the cabin class and the Georgia in the open class. The cruiser race on Thursday afternoon was a duel between the former champion Wemco II and the new Toledo flagship Hussar II. Off the wind Wemco held the lead but it breezed up on the windward leg and Hussar showed a clean stern to the bunch, winning with time to spare.

On Friday night the winners of 1st, 2nd, and 3rd places in each race received silver, bronze and aluminum medallions respectively in addition to flags and special trophies. Having no intrinsic values those mementos are held as keepsakes around the circuit and have been aptly called "Rathbone's monuments." Commodore Smedley's new auxiliary yawl Vanessa was the flagship of the fleet in 1904.

The 1904 administration ended at Detroit on December 5th of that year in a blaze of glory. The annual meeting was held at the Russel House followed by a banquet which, if anything, exceeded the one given in the same hall the preceding spring. The room was transformed into a Japanese garden and the waiters

were painted up and dressed in the costume of the little brown men who have been making so much history in the far East.

By far the most important action was the adoption of the report of the rules committee providing for new restrictions in the 20, 18 and 16-foot classes. These were moulded after the form of the Y. R. U. restricted classes requiring similar measurements for water line, beam, cross section area, draft, sail area, etc. A table of scantlings for each of the classes was included. The Massachusetts Bay knockabouts were eligible to race in the 18-foot class as existing yachts, as were also the 18-footers of the I-L. Y. A. which had previously raced in the class under the 1902 restrictions. The same applied to the 16-foot class for 15-footers of the Atlantic Coast.

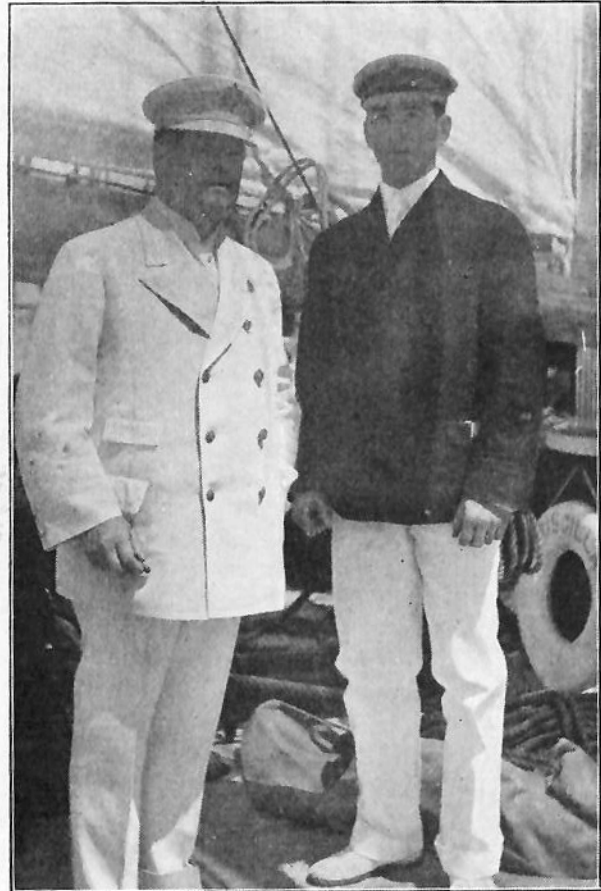
### 1905 OFFICERS

Commodore .....GEORGE H. WORTHINGTON..Cleveland Y. C.  
Vice-Commodore .....FRANKLIN H. WALKER.....  
.....Detroit Country Club.  
Rear-Commodore .....JOHN F. CRAIG.....Toledo Y. C.  
Secretary-Treasurer ....W. J. BRODIE .....L. Y. C.  
Fleet Captain .....N. WARREN.....D. B. C. Y.  
Measurer .....C. P. SIEDER.....D. Y. C.  
Chr. Regatta Com.....M. B. VORCE.....L. Y. C.



WHEN Commodore Worthington took up his work at the Spring meeting of the Association in Cleveland, he found a depleted treasury and the records in such shape that a complete reorganization was necessary. It was up to Cleveland to pull the I-L. Y. A. out of the hole and the committees went to work with a will. The annual regatta was scheduled for Put in Bay July 24-29, 1905.

As a result of the new restrictions, one boat was built for the 18-foot class, Winina, owned by Messrs. Peterson, Davies and Fuller, of the Lakewood Yacht Club. She was designed by Small Bros after the model of the famous Hayseed and her measurements conformed to the limit of the restrictions as follows:



COMMODORE WORTHINGTON  
and Fleet Captain Warren, at Put in Bay, 1905

Length over all 28 feet 8 inches, load water line 18 feet, beam 7 feet 9 inches, draft 4 feet 6 inches, area midship section 7.5 square feet, sail area 514 square feet, displacement 4350 pounds. She is the only restricted 18-footer ever built for and raced in the I-L. Y. A class from 1902 to the present time. It was proven later that the rules were at fault in that she was too short on deck with too much beam and too little draft to hold her own against the Massachusetts Bay knockabouts that have since been added to the Lake Erie fleet. However, she is a good, wholesome boat and a credit to her present owner, Mr. Peterson.

The annual regatta opened Tuesday, July 25th, with races for all classes over the 12-mile triangular course. Sultana finished

racing for good, while Ventura was not in commission. Spray was in better form even than the year before, taking the first race from Ste. Claire by six minutes, with Rooster in third position. The new 20-foot restricted class found Mermaid of Detroit an easy winner over Gracie and Lotus of Toledo.

The 18-foot class consisted of the 18-foot centerboarder, Wrinkle, Winina and Malillian of Lakewood and Scud of Toledo. Malillian was a Massachusetts Bay knockabout purchased that Spring by Geo. W. Hotchkiss to race against the new Lakewood craft. She proved a winner in the first race, defeating Wrinkle over 7 minutes and making all her gains on the windward leg where her performance was a revelation. Winina was a poor



Squadron Sail at Put in Bay July 27, 1905

12 minutes ahead of City of the Straits in the 40-foot class, Enright being disqualified for starting on the preparatory gun. The 30-foot race went to Echota, Cadillac having been sold to New Orleans the previous fall; Juanita of Toledo and Orinda of Lakewood finished in order. Elsie, a new Small Bros. cruiser owned by R. P. Mathias of the T. Y. C., won in the 25-foot class after a close race with Squall, sailed by Frank King of the same club. Chloris was not a contender this year, being out of commission at Rocky River.

From a fleet of six boats in 1904, the 21-foot class dwindled to three entries. Eyota and Pirate, despite the efforts of their owners, could not be measured into the class and were out of the

third, with Scud last.

The 16-foot race brought another new boat to the class, Shake, of the Country Club, built to the new restrictions from Small Bros., design and a pocket edition of Winina, except that she was a centerboard craft. Sella II, another new restricted 16-footer built by the Luedtke boys at Toledo, was a poor second over 20 minutes behind Shake with Helen of Detroit third.

The race on Wednesday was sailed over a windward and leeward course of 10 miles. Like the first day, there was but little wind and conditions were favorable to light weather craft. Sultana repeated her victory in the 40-foot class, Enright second and City of the Straits third. Echota had an easy time in the 30-



WININA—(Renamed Yankee)

The first restricted 18-footer ever built for the I-L. Y. A. Class, 1905

foot class, beating Commodore Gardner, the old Cleveland cutter, almost half an hour. In the 25-foot class Elsie won an easy victory over the old Detroit yawl Minerva, the latter just managing to finish ahead of the Toledo entry, Squall.

Ste. Claire had her turn in the 21-foot class, beating Spray 25 seconds and winning the race on the windward leg. Rooster of Cleveland was a poor third. Mermaid of Detroit had practically a sail-over in the 20-foot class, as her contestants were not in sight when she finished. In the 18-foot class Wrinkle came back and gave the Cleveland boats a beating, winning over Malillian by 4 minutes 17 seconds. Winina was third 10 minutes behind Wrinkle, with Penguin, a new entry from Buffalo, 6 minutes behind Winina. Shake, of Detroit, was the only one to finish in the 16-foot class as the other boats were disqualified. This race brought a protest from Malillian questioning the right of Wrinkle to race in the 18-foot class, as she did not, in any sense, comply with the new restrictions. She was permitted to continue because of the fact that she had raced in the class in 1903 when the old rules were in effect.

The free-for-all race for the Victory cup on Friday over the 12-mile triangular course was also considered a class race, the boats starting together at 8:30. Sultana made it three straight in the 40-foot class, while Echota had a sail-over for the 30-foot prize. Elsie cinched her claim to the 25-foot championship, defeating a new entry from Erie, Amette, which had arrived during the week. Spray won easily in the 21-foot class, making the fastest time of the fleet over the course and annexing a heat on the Victory cup. Ste. Claire was a poor second and Rooster third. Mermaid took a sail-over in the 20-foot class and Wrinkle won the deciding race among the 18-footers, beating Winina almost 15 minutes. Shake finished alone in the 16-foot class and proved beyond a doubt her supremacy in the I-L. Y. A. The wind was light and fluky and the race was mostly reaching and running.

Two classes of power boats raced on Wednesday, Restless winning the big race and Capitola Jr. the race for power dinghys. The cruiser race for the Commodore's cup on Thursday

was won by City of the Straits, with Sultana second and Enright third.

The winners during the week proved their supremacy in light weather as there was hardly a fair sailing breeze during the whole series. The prizes distributed Friday evening consisted of silver cups and flags for the champions and merchandise prizes for the place winners. These consisted of articles of equipment, such as side lights, barometers, anchors, etc.—enough junk to fit out a good-sized fleet. The *Toledo Blade*, the *Detroit Free Press* and the *Cleveland Press* gave their usual cups for the 25, 21 and 18-foot classes respectively.

Commodore Worthington's schooner yacht *Priscilla* was the flagship of the fleet in 1905 and the hospitality afforded the yachtsmen will long be remembered by those in attendance. The Commodore held formal and informal receptions during the entire week and in every way did much to contribute to the success of the regatta. Furthermore, he instituted two innovations in I-L. Y. A. entertainment that proved extremely popular and brought hundreds of visitors to the Bay during race week. The first was the presence of Verdi's band of Cleveland giving concerts from the deck of *Priscilla*, and making the anchorage ring with their music. The display of fireworks on Wednesday evening was the other feature that made the week a memorable one. They were fired from a huge float in the Bay and presented a gorgeous and beautiful spectacle.

Before closing the 1905 record, we should refer to the careful, systematic and altogether splendid management of the Sec'y-Treas., W. J. Brodie. It was indeed a financial proposition to make both ends meet and it stands to the credit of the administration this year that it paid all of its expenses and a considerable portion of the debt. Commodore Worthington assumed a great deal of the burden personally by taking care of much incidental expense which is always a drain on the regular income. Later on we shall see that Secretary Grasser made an equally brilliant record and that by hard work and good judgment the Association is now in a prosperous condition.



JOHN BARTH  
Chairman Entertainment Committee, 1905

1906  
OFFICERS

Commodore, Jno. F. Craig.....	T. Y. C.
Vice-Commodore, F. W. Fletcher.....	T. B. Y. A.
Rear-Commodore, C. G. Jennings.....	C. C.
Sec'y-Treas. Jos. M. Grasser.....	T. Y. C.
Fleet Captain, Geo. E. Hardy.....	T. Y. C.
Measurer, R. Luedtke.....	T. Y. C.
Ch. Regatta Committee, W. F. Schumacher.....	T. Y. C.



HE annual meeting was held at Cleveland December 2, 1905, when the officers for the ensuing year were elected. About the most important business transacted was a change in the rules and restrictions for the small classes, the discussion being entered into with great vigor by Toledo and Cleveland representatives in the 18 and 16-foot classes. Cleveland secured an amendment requiring cabins on 18-footers and extending the overhang from 60 to 70 per cent of the water line length.

The Toledo delegates insisted that the '05 rules precluded the building of a centerboard boat that would equal the keel boat in speed and other qualities. They carried through an amendment requiring that the area of the immersed cross-section be determined by measuring the area bounded by the outside of the hull and included between the load water-line plane and a line parallel to it and intersecting the keel one inch below the lowest point of the interior of the hull at the measured section.

Because of this change the number of square feet required in the cross section was cut down in the 20-foot class from 9.25 to 8; in the 18-foot class from 7.5 to 6.25 and in the 16-foot class from 5.25 to 4.5. Likewise the sail areas were reduced to 600, 500 and 400 square feet respectively. This meant that the displacement of the keel boat below the rabbet line was not credited to the design. The revision was considered a victory for the centerboard men over the keel adherents. It may be said right here that although these amended rules stood during the year 1906, only one boat, the 16-footer Gamble, put up by the Country Club syndicate,



JOHN F. CRAIG OF TOLEDO  
Commodore of the Inter-Lake Yachting Association, 1906



was built to the rule. In some respects she was faster than Shake, but was badly beaten by Hoorah, an imported 15-footer from Massachusetts Bay.

Commodore Craig called his organization meeting at Toledo January 27, 1906. The minutes of this meeting are mostly taken up with the effort made by delegate Vorce of the Lakewood Yacht Club to have the rules just referred to set aside. He was not sustained though the fight was warm and the smoke pretty thick for a while.

The cat-boat class was originated at this meeting and the rules committee was instructed to draw up suitable restrictions. Put in Bay was selected as the place for the annual regatta. The date set was the week of July 15-21.

Fortunately the records of the 1906 regatta as well as those of the previous year are well preserved so that we do not feel called upon to go into great detail. Conditions were miserable for good racing as the only real sailing breeze of the week was had during the first part of the race on Friday. Despite this fact the entries were representative of the best boats in the Association and the 21-foot class especially showed a decided improvement over the previous year.

Three new boats had been built probably on account of a very energetic campaign on the part of the Lipton cup committee at Chicago. The Country Club had commissioned B. B. Crowninshield to design a centerboard boat to the limit of the class which they handicapped with the name, Otsiketá. Toledo went to Small Bros. for a compromise centerboard keel drawing four feet of water and carrying almost the limit of sail. Commodores Worthington and Gardner, J. H. Wade and M. A. Bradley of Cleveland, formed a syndicate with the result that they also brought out a Small Bros. boat built by Lawley with a little more draft than Toledo and a small board housing in the keel. She was called after her home city and club and was the pick of the new fleet. Having been built on a carte blanche order commensurate with the wealth and generosity of her owners, she was about the finest racing craft that ever came to the fresh water lakes.

A new Massachusetts Bay 18-footer had been brought to Lake Erie by Ralph L. Fuller of the Lakewood Y. C. that had proven faster than the '05 boats. She was the Aladdin, a Crowninshield product of 1904. Toledo built Beener intended as an 18-footer but raced in the 20-foot class. The last and probably the most notable of the new small boats was another Massachusetts importation, Hoorah, purchased by Geo. Q. Hall, of the Lakewood Y. C.

The first race was sailed Tuesday, July 17, over the 12-mile triangular course. Enright and Ethel, the old Toledo cruisers, finished in order in the 40-foot class. Juanita, now owned by Mr. Bortree of Toledo, won a close race in the 30-foot class, from Commodore Gardner with West Wind, the Buffalo schooner, bringing up the rear. Chloris was in commission again after a year's absence and had an easy time disposing of Elsie of Toledo in the 25-foot class, with Vinco of Lakewood an also-ran.

Six 21-footers lined up for the start at 6:30. Pitted against the new boats mentioned were Spray, Ste. Claire and Rooster, now owned by E. R. Rickersburg of the Lakewood Y. C. Through good judgment or good luck in getting a favorable slant of wind, Toledo defeated Spray almost 5 minutes. Cleveland was leading at the first mark with Spray and Rooster close behind. Toledo in the rear was allowed to split tacks and when the boats converged on the second leg, she was so far ahead that the race was over. Spray was second 12 minutes ahead of Cleveland with Otsiketá, Rooster and Ste. Claire in order. The 18-foot race was won by Wrinkle by 5 minutes over Aladdin, with Yankee and Malillian bringing up the rear. Hoorah handed Sella II a decisive defeat in the 16-foot class with Tramp, a new Lakewood boat, in third place. The wind was light and fluky and the entire contest was a guessing match from start to finish.

Although the weather on the first day had not been satisfactory, it was ideal compared to conditions in the windward and leeward race on Wednesday. There was a fine northeast breeze in the early morning and the starting line was established northwest of Rattlesnake Island in order to give a clear course to windward and return. The 21-footers started first at 9:10. Before they had covered half the distance to the first mark they were becalmed

and from then on until 2 o'clock in the afternoon they lay sweltering in the hot sun. A light southwest breeze came across from the Ohio shore and Spray beat her way back to the finish line, getting the gun at 6 minutes after 4. Cleveland was second an hour later with Toledo and Ste. Claire in order a few minutes behind Cleveland. Otsiketa and Rooster had withdrawn and taken a tow for home. With the exception of Wrinkle all of the boats in the other classes finished after 5 o'clock. Ethel beat Enright in the 40-foot class, while West Wind had an eyelash victory over Juanita in the 30's. Chloris beat Elsie half an hour and Tattoo took the 20-foot race from Beener. Wrinkle annexed her second victory in the 18-foot class, with Malillian second and Aladdin third. Hoorah again beat Sella and cinched the championship for 16-footers. In all the history of the Association, we find nothing that exactly compares with this fiasco of the '06 meet.

The Victory cup race on Friday was the best contest of the week. A fair west wind was blowing at the start, which increased on the second leg and held long enough for the leading boats to cross the finish line. Spray had an easy victory in the 21-foot class with Otsiketa, Cleveland, Toledo and Ste. Claire in order. Enright took the deciding race in the 40-foot class, with Commodore Gardner a winner in the 30's. Chloris made it three straight against Elsie with 10 minutes to spare. The 20-foot race again went to Tattoo, with Beener second, and Teal, another new Lakewood boat, in third place. The 18-foot race was Wrinkle's by more than half an hour over Yankee, with Aladdin, sailed by her new owner, W. F. Nash, in third place. Hoorah beat Sella again in the 16-foot class. Spray was awarded the Victory cup, which gave her two heats on the trophy.

The power boat races were run on Thursday, the boats being divided into three classes and rated under A. P. B. A. rules. Bab won in the cruiser class, defeating Wilanna, Thelma, Naiad, Marion and Madeline. In the small class Alice H was the only entry, while Ecce of Toledo enjoyed the same distinction in the auto boat division. Puritana Jr. won the dinghy race.

The first cat-boat race ever held at Put in Bay was also sailed on Thursday and was won by Frank Frey from his Toledo com-



FRANK W. FLETCHER OF TAWAS  
Commodore of the Inter-Lake Yachting Association, 1907

petitors, Rawson and Wilson. The prizes distributed on Friday evening were similar to those of the year before, consisting of cups, flags and merchandise.

Commodore Craig had followed Commodore Worthington's example of the year before and had provided a band and fireworks by way of entertainment. Though not as successful as the previous



JOS. M. GRASSER  
Sec'y-Treas. I.L. Y. A., 1906-07-08



C. A. RUSSELL  
Rear Com. I-L. Y. A., 1907

year, due to the poor weather and unsatisfactory conditions on the race course, the meet was well attended and enjoyed by the yachtsmen. The flagship in 1906 was the steam yacht Edith.

### 1907 OFFICERS

Commodore, F. W. Fletcher.....T. B. Y. A.  
Vice-Commodore, M. B. Vorce.....L. Y. C.

Rear-Commodore, C. A. Russel.....T. Y. C.  
Sec'y-Treas., Jos. M. Grasser.....T. Y. C.  
Fleet Captain, C. P. Sieder.....D. Y. C.  
Measurer, R. A. Luedtke.....T. Y. C.  
Chr. Regatta Committee, G. H. Gardner.....L. Y. C.



HE most important record of the annual meeting held at the Toledo Yacht Club, December 29, 1906, was the report of Sec'y-Treas. Grasser showing the Association out of debt with a balance of \$6.57 in the bank. As I have said before, the administration under Commodore Craig is entitled to the greatest credit for making such satisfactory showing.

At the Fall meeting of the Yacht Racing Union, the universal rule in its complicated entirety was adopted, bringing on to the Great Lakes the fourth complete change in rules since our story began. Dr. Jennings, who was an active supporter of the new rules, sought its adoption before the Inter-Lake Association and was successful. It meant that with the exception of the 21, 18 and 16-foot and cat-boat classes, the entire fleet of the I-L. Y. A. would be rated and raced under the new formula. As that rule is now in force and is included in this volume we shall not mention it further.

The next meeting of the Association was held at the Wayne Hotel, Detroit, February 2, 1907. Commodore Fletcher, in appointing his committees, turned the racing end over to Cleveland yachtsmen and made them responsible for its success. The chairman and active members came from the Lakewood Y. C. and the record of their official work goes down in history as the bright particular feature of the 1907 regatta. The entertainment committee was made up of members of the Detroit B. C. Y. A power boat committee of three was added to the list as a permanent fixture. Steps were taken to admit the Buckeye Lake Yacht Club to membership in the Association. After some discussion relative to a generous offer from Cedar Point it was decided to hold the 1907 meeting at Put in Bay July 15-21. The new Detroit Motor Boat Club joined the Association in 1907.

The growth of the Association in 1907 was most apparent in the 18-foot class built up by the Lakewood Yacht Club until

it numbered eight boats when the '07 season opened. Hayseed, champion of Massachusetts Bay, had been purchased by Walter C. Baker the fall before and she was soon followed to fresh water by the present champion Bonitwo, bought by Geo. W. Hotchkiss soon after he sold Malillian. In the spring of 1907 W. F. Nash imported the Boardman 18-footer Fudge from Massachusetts Bay, while H. R. Dryfoos, a new member of the Lakewood fraternity, secured Hayseed II.

Borealis, the Hanley 21-footer built the year before for a syndicate headed by Commodore Fletcher, was a contender this season in the races of the Association. The list of new boats which competed at Put in Bay is complete when we mention Oriole, a new 16-footer built by Herman Krause of the Lakewood Yacht Club, and Mascotte, a Toledo 16-footer brought out by C. W. Quetschke.

The 1907 regatta was notable for many reasons. In the first place it marked the return of the Association to cash prizes, putting the sport on a sound, definite basis where it is to be hoped it will remain for a long time to come. The regatta committee in making up the schedule endeavored to study the results of past seasons with a view of improving conditions rather than copying verbatim the program of the year before. The first thing they did was to put time limits on all the races and to shorten the triangular course to 9 miles and the windward and leeward to 8 miles. Furthermore, they scheduled five races instead of three, believing that boats intending to race, went to Put in Bay for that purpose and would rather be on the course than loafing around. The result of this schedule caused some criticism that there was too much racing and no time for entertainment. It seems now to be the consensus of opinion that four races is the happy medium, with an open day for a squadron sail and other sports.

Two classes under the universal rule were called for on the program, but the committee could only find seven boats that were not included in the restricted classes and they were all grouped together. The owners gave in their ratings without any certificate of measurement with the result that the committee had to waive its announced purpose and let them race on a "catch as catch can" basis. J. H. Fearnside of Hamilton entered the old Canada's cup

champion, Invader, and was compelled to wire home for blue-prints which arrived in time for him to take the championship. In comparison with the systematic competition in the restricted classes, the application of the universal rule to the other boats was ludicrous to say the least.



INVADER OF HAMILTON

First Universal Rule Champion on Lake Erie, 1907

The racing began on Monday morning, July 15, with conditions almost perfect for the contest. The wind was S. S. W., giving the boats a spinnaker run, a beat and a reach on the three legs of the triangle. Promptly at 8:30 the 21-foot class was sent away. Spray,

sailed by her new owner, Fred A. Price of the Columbia Y. C. Chicago, took the lead on the first leg and was never headed. The race for second place was spirited between Borealis and Ste. Claire, the former excelling in the windward work and finishing a few seconds ahead of the old champion. Ste. Claire was entered by the Monroe Yacht Club, having been bought early in the season. Otsi-



MYRON B. VORCE  
Vice-Com. I-L. Y. A., 1907



C. G. JENNINGS  
Vice-Com. I-L. Y. A., 1908

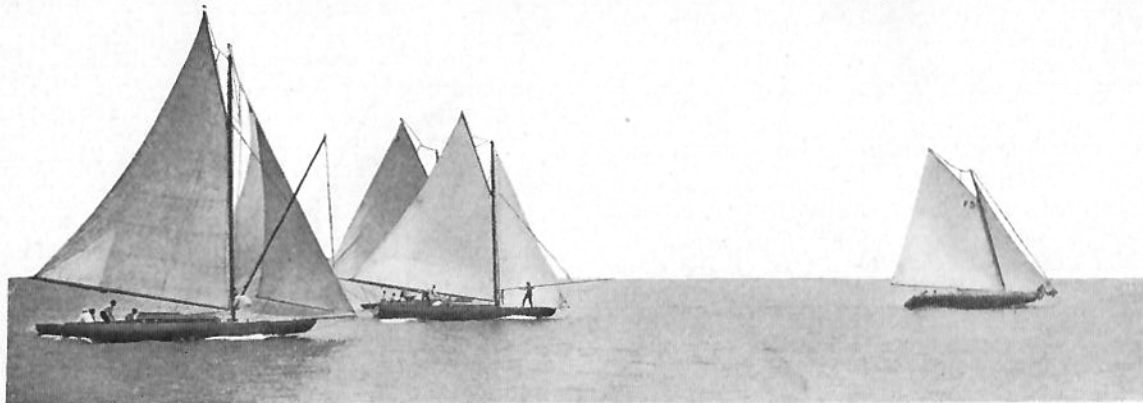
keta was fourth and Cleveland last. Invader lead the way in the universal class followed by Elsie and Enright but when corrected times were figured out it was found that Mildred S, a Detroit 20-footer sailed by Fleet Captain Sieder, was entitled to first place. Elsie, Wrinkle, Invader, Mermaid, Enright and Ethel were placed in order.

The 18-foot class was a spirited contest between the Lakewood knockabouts. Bonitwo took the lead on the first leg and held her advantage by a narrow margin over Fannette (ex Hayseed II) the latter finishing 31 seconds behind the champion. Fudge was third followed by Yankee, Aladdin and Malillian. Hoorah was matched against Gamble in the 16-foot class and these boats had a battle royal. The Lakewood craft beat the Country Club entry 17 seconds with Sella II and Mascotte some distance behind the leaders. Frank Frey with Lucille won the cat-boat race, with Holy-stone, sailed by Joe Grasser, second, and Maud third.

The race on Tuesday morning was to windward 4 miles and return. The S. W. wind took the boats toward Rattlesnake Island, where the starting line was established. It was a very light day with streaks and slants all over the course, though the leaders all managed to finish within the time limit. Spray was again invincible in the 21-foot class, defeating Otsiketa 7 minutes with Borealis 3 minutes later. Cleveland was a poor fourth and Ste. Claire last. Invader, scratch boat in the universal class, showed enough speed to win on corrected time with Enright second and Elsie third. The small boats, Mermaid, Wrinkle and Mildred S, were distanced and brought up the rear.

Bonitwo, in the 18-foot class, showed decided superiority on the windward leg and took down her second victory. She had ten minutes advantage at the turning stake and her race home was against the three hour time limit, which she beat by 41 seconds. Fannette, Malillian and Fudge finished in a bunch 5 minutes behind Bonitwo and in that order. Yankee was fifth and Aladdin last. In the 16-foot class Sella II upset all calculations by giving Hoorah a 5-minute beating. Gamble was a poor third, with Oriole and Mascotte in order.

On Wednesday the triangular course was repeated in a breeze that was strong and puffy from the S. W. at the start. It lightened considerably but held true to the finish. The 21-foot race was a close contest between Spray and Cleveland, the former winning out by less than a minute. Ste. Claire was third, with Otsiketa and Borealis sailing practically a dead heat for fourth place. The former won out by one second. Although Invader finished first in



21-Footers Racing at Put in Bay

the universal class, she was defeated by Enright on corrected time with Elsie third and Ethel, Mildred S and Mermaid in order. Wrinkle did not start.

The 18-foot race went to the old champion, Malillian, with Fudge 4 minutes later in second place. Bonitwo and Fannette engaged in a private battle on the second leg which gave Malillian her chance to clean up. The former finished third and Fannette brought up the rear. Hoorah beat Sella almost a minute, making her gain on the windward leg. Gamble was a poor third 9 minutes behind Sella with Oriole and Mascotte almost a mile astern. The cat-boats finished in the same order as on the first day. The windward and leeward race was sailed on Thursday under about the same conditions as Tuesday's race. Spray walloped the 21-foot fleet again and the only race in this class was for second place. Borealis worked through the fleet from a poor position, and was far enough ahead at the windward mark to hold her advantage to the finish. Ste. Claire finished in order with less than a minute separating the three boats.

It was a battle between Bonitwo and Fudge from the start in the 18-foot class, the Crowninshield craft showing her superiority

on the windward leg. Fudge gained on the run home but was a minute behind at the finish. Fannette was third 9 minutes behind Bonitwo with Malillian and Yankee in order. In the 16-foot class Hoorah handed Sella an 8-minute defeat, while Oriole, although 10 minutes behind Sella, finished 5 minutes ahead of Gamble. Mascotte withdrew. The wind on Thursday was N. N. W.

The final race of the regatta was sailed on Friday over the 9-mile triangle. It had been decided by unanimous consent that the winner of the 21-foot race should take the Victory cup and Spray fulfilled the hopes of the yachtsmen by taking the third and deciding heat on that trophy. There was a good sailing breeze from the S. W. at the start but it died out about 11 o'clock, leaving the smaller classes becalmed within sight of the finish line. Cleveland took second place, defeating Borealis by a narrow margin, with Otsiketa in last position. Ste. Claire was disqualified for ramming the Fletcher boat on the second leg. Enright won her second victory in the universal class with Wrinkle second and Invader third. Elsie, Ethel, Mermaid and Mildred S finished in order.

The smaller class races postponed from the morning were started at 2:30 in the afternoon, and were sailed over the cat-boat

triangle of 4½ miles. The 18-footers had the most exciting race of the week, Fannette winning over Bonitwo by a quarter of a length. Yankee was third, Fudge fourth and Malillian last. Hoorah took the last race in the 16-foot class, with Gamble second, Sella third and Mascotte last. Frank Frey made it three straight for the cat-boat money, with the other boats in the same positions as before.

Two classes of power boats raced on Wednesday afternoon under the A. P. B. A. rules. Arrow, of Bay City, won in the auto boat class with DeMooy, of Cleveland, P. D. Q. and Little Grayling of Detroit, Stormy Petrel of Put in Bay, Ecce of Toledo and Vim of Sandusky, in order on corrected time. The course was 12 miles, the winner's time being 37.25. Wilanna won an easy victory over Wistaria in the cruiser class, with Viking, the old 40-foot sloop converted into a power cruiser, bringing up the rear. The free-for-all race for auto boats was run on Thursday, Arrow winning again by a narrow margin over Weasel, a new Lakewood entry. DeMooy was third.

On Friday night the prizes were distributed, consisting of a silver cup and flag to the class champion and three cash prizes for each race. These aggregated \$125 in the 21-foot class, \$125 in the Universal class, \$100 in the 18-foot class, \$75 in the 16-foot class, \$45 in the auto boat class, \$30 in the cruiser class and \$30 for the cat-boats, a total of \$530.00. From a racing standpoint the 1907 regatta was a success and good weather favored the meeting. Commodore Fletcher's flagship in 1907 was the steam yacht Winyah



**HOORAH**

Champion I-L. Y. A. 16-Footer, 1906-07

## 1908 OFFICERS

Commodore, Geo. H. Worthington.....	C. Y. C.
Vice-Commodore, C. G. Jennings.....	C. C.
Rear-Commodore, Frank R. Frey.....	T. Y. C.
Sec'y-Treas, Jos. M. Grasser.....	T. Y. C.
Fleet Captain, O. P. DeMars.....	C. Y. C.
Measurer, Carlton Wilby.....	D. M. B. C.
Chr. Regatta Committee, G. H. Gardner.....	L. Y. C.



THE last annual meeting of the I-L. Y. A. was held at Detroit, December 14, 1907. The new home of the Country Club was thrown open and the delegates and visiting yachtsmen were handsomely entertained. Myron B. Vorce was unanimously elected Commodore of the I-L. Y. A. but immediately declined the honor. It was a marked compliment to him for long and efficient service in the interests of the sport.

A motion to permit new boats in the 16 and 18-foot classes to carry hollow spars met with favor and the 16-foot class was put upon a strict amateur basis to conform to the 18-foot restrictions.

The scantling table of the Y. R. U. for the universal rule classes was adopted.

Commodore Worthington called his organization meeting at Cleveland February 8, 1908. He announced the appointment of some new committees in order to more properly divide the work of conducting the regatta. The power boat committee was increased to five members. In addition the following were added to the regular quota: Dinghy race, cruiser race, banquet, squadron sail, aquatic sports, annual ball and program committees. It was moved to invite the yachtsmen from Lakes Michigan and Ontario to participate in the annual regatta and immediate plans made to secure their attendance. Put in Bay was chosen for the annual regatta and the dates set for July 19-25. The Gordon Park Motor Boat Club of Cleveland joined the Association at this meeting.

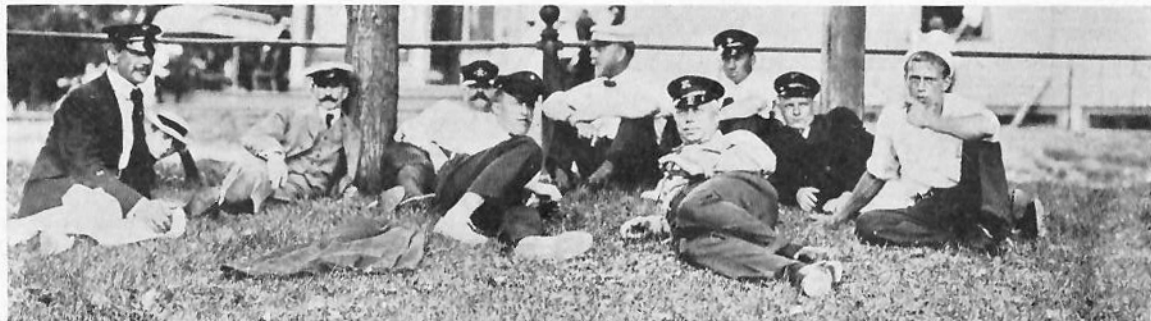
We have at last reached the end of our story and the history of the Inter-Lake Yachting Association from now on is a matter for future record. Before closing, let me at least mention the names of those who have, from year to year, donated to the prize list for the regattas and who have helped largely toward the success of the I-L. Y. A.

Among the newspapers the *Toledo Blade* and *Cleveland Press* have been constant patrons for many years. The *Toledo News Bee* and the *Detroit Free Press* gave liberally up to a few years ago

but have since drifted away from yachting affairs. Among the magazines, *Sail and Sweep* during its life gave some handsome trophies, while *Fore'N'Aft* and *Boating* have since contributed to the prize lockers. Last year the Matthews Boat Co. gave a silver punch bowl to the power cruiser class, the Smalley Motor Co. a cup to the auto boats and the Jennison Hardware Co. a silk flag.

The most liberal individual donor has been Commodore W. R. Huntington who has put up a cruiser cup every year since 1902. Commodore Geo. W. Gardner contributed a perpetual trophy to the 21-foot class in 1906.

One thought has become tremendously impressed upon the writer in preparing this brief sketch—the great, permanent good which the Inter-Lake Yachting Association has done for yachting in and about Lake Erie. It encourages intercommunication and friendly rivalry between the clubs in the circuit and tends to promote a deeper, broader spirit of fraternalism among devotees of the sport. The members of the Association could do nothing greater for yachting on Lake Erie and vicinity than to co-operate more seriously in this work and begin preparations at once to erect a permanent club house at Put in Bay. This would seem to be the next logical step in uniting more closely the scattered interests and would give the Association a home, centrally located in the most beautiful sailing waters on the Great Lakes.



Scene near Headquarters Put in Bay, 1907



# U. S. Y. A. RACING RULES

## RULE I.

### Management of Races.

All races, and all yachts sailing therein, shall be under the direction of the Regatta Committee of the Association or Club under whose auspices the races are being sailed. All matters shall be subject to their approval and control, and all doubts, questions and disputes which shall arise shall be subject to their decision. Their decision shall be based upon these rules as far as they apply, but as no rules can be devised capable of meeting every incident and accident of sailing, the Regatta Committee should keep in view the ordinary customs of the sea, and discourage all attempts to win a race by other means than fair sailing and superior skill and speed. The decision of the Regatta Committee shall be final, unless they think fit, on the application of the parties interested, or for other reasons, to refer the questions at issue for the decision of the Council of this Union, whose decision shall be final. No member of the Regatta Committee or Council shall take part in the discussion or decision upon any disputed question in which he is interested.

## RULE II.

### Measurement for Classification and Time Allowance.

Yachts shall be rated for classification and time allowance according to the following formula:

$$\text{Rating Measurement} = \frac{L \sqrt{S A}}{5.5 \sqrt[3]{D}} \left. \begin{array}{l} \text{Length multiplied by square root of} \\ \text{sail area, divided by 5.5 times cube} \\ \text{root of displacement.} \end{array} \right\}$$

The result is the measurement for classification and time allowance. The measurements shall be obtained as follows:

#### Length.

L, measured on a line parallel with the middle fore-and-aft vertical plane at a distance from it equal to one-quarter of the greatest beam (B) at the load water-line, and one-tenth of this beam (B/10) above the load water-line; and in case there are any notches, jogs, curves, or angles, at or near the plane of measurement, L shall be taken on a fair line bridging such notches, curves, jogs or angles.

#### Sail Area

The Measurer shall measure the spars and calculate the sail area in the following manner, and the square root of this area shall be the  $\sqrt{S A}$  in the rating formula:—

At the option of the owner the outer points of measurement on the boom and gaff may be black bands, beyond the inner edges of which the sail shall not be extended.

#### MAINSAIL.

A—Measured from top of boom at end to under side of gaff at end; any extension of gaff or boom to be considered part of gaff or boom, and to be extended to its full limit when measured.

B—Perpendicular to A, measured to under side of gaff close in to mast.

C—Measured from top of boom at end, or any extension thereof, when extended to its full limit to under side of gaff close in to mast.

D—Perpendicular to C, measured in to mast in a line with top of boom, or to tack cringle of mainsail if below top of boom.

To find the area of the mainsail: Multiply A by B and C by D and add the two products together and divide by 2.

#### FORMULA.

The following formula can be used at the option of the measurer:

To find the area of any triangle of which the three sides are given:

In a triangle having sides  $a$ ,  $b$  and  $c$ : Let  $S$  = Semi-Perimeter, i. e., one-half the sum of the three sides of  $a + b + c$ .

$$2$$

Then area of triangle =  $\sqrt{S(S-a)(S-b)(S-c)}$ .

#### YARD TOPSAIL OR CLUB TOPSAIL.

E—Measured from upper side of gaff close in to mast to pin of sheave for topsail sheet, or to lacing-hole in jackyard.

F—Perpendicular to E, measured to lacing-hole in yard.

G—From lacing-hole to lacing-hole in yard.

H—Perpendicular to G, measured to pin of sheave for topsail sheet in gaff, or to lacing-hole in jackyard.

To find the area of yard topsail or club topsail: Multiply E by F and G by H and add the two products together and divide by two.

#### JIB HEADER.

K—Measured from top of gaff close in to mast to pin of halyard sheave in topmast.

L—Perpendicular to K, measured to pin of topsail sheet sheave in gaff, or to lacing-hole in jackyard.

To find the area of jib header: Multiply K by L and divide the product by 2.

#### HEADSAILS.

I—A perpendicular taken along the after-side of the mast from the upper side of the sheave of the highest halyard block or sheave on the mast or topmast to the upper side of the boom when resting on the saddle or on the lowest part of the gooseneck; the distance of which point above the fair line of solid bulwarks shall be recorded by the Measurer.

In pole-masted yachts which carry the upper halyard block on a pennant, the upper point of measurement shall be the point at which the pennant is fastened to the mast.

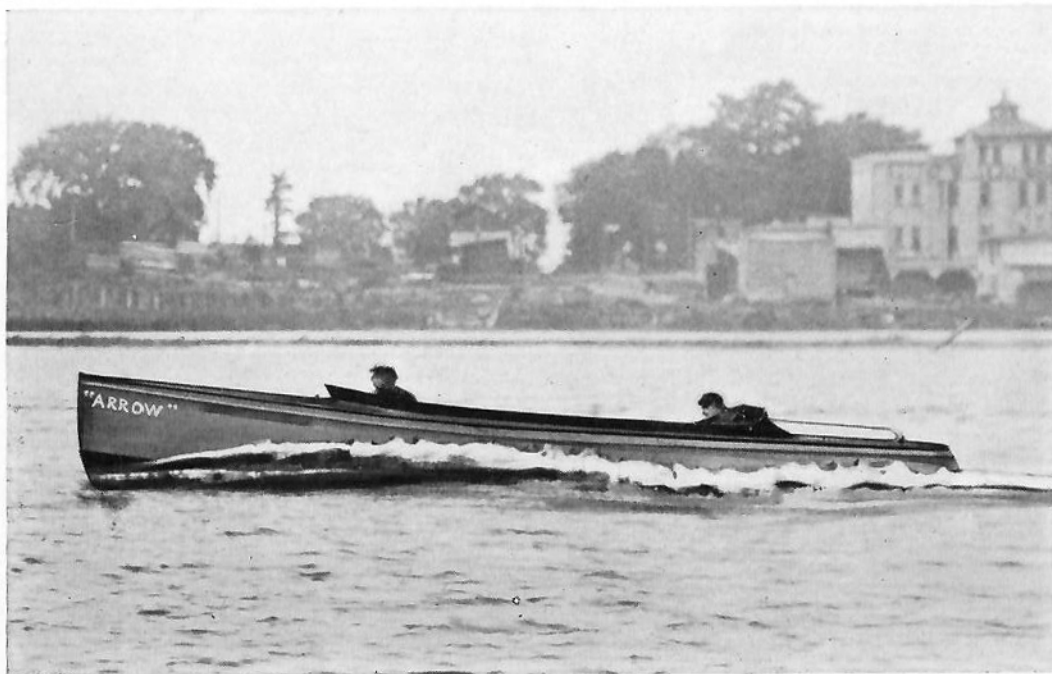
In the case of a schooner I shall be taken along the after-side of the foremast from the upper side of the sheave of the highest halyard block or sheave on the topmast to a point at the same distance above the fair line of solid bulwarks at the foremast, as the lower point of measurement of I is at the mainmast.

J—The base J to be measured from the foreside of mast to where the line of the luff of the foremost headsail when extended cuts the bowsprit, other spar, hull, etc., as the case may be.

In yachts of 27 feet rating and over, if the distance from the center fore-and-aft line of the mast to the outer end of the spinnaker boom, when in position, exceeds the distance from the foreside of the mast to the bowsprit

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WATER STREET, FOOT OF FOURTEENTH



*The Arrow  
carried off all  
prizes both in  
the Free-for-all  
and Handicap  
Races at  
Put-in-Bay  
in 1906.*

end (where cut by the line of the luff of the foremost headsail), the excess shall be added to the base of the fore-triangle. In the case of a schooner, the base J shall be measured from the foremast, but if the main or longest spinnaker boom exceeds the before-mentioned distance, the excess shall be added to the base J.

In yachts under 27 feet rating, the spinnaker boom may be 40 per cent of the length of the base line measured from the foremost point of measurement on the bowsprit, other spar or hull, as the case may be, in a straight line to the extreme afterpoint of measurement. Any excess of spinnaker boom over such 40 per cent shall be added to the base J.

The area of the headsails shall be obtained by multiplying J by I, dividing by 2 and taking 90 per cent of the result.

$$\text{Formula: } 0.90 \frac{I \times J}{2}$$

#### FORESAIL OF SCHOONERS.

A—Measured from fore side of mainmast (in a line with main boom gooseneck) to under side of gaff at end; any extension of gaff to be considered part of gaff, and to be extended, when measured to its full limit.

B—Perpendicular to A, measured to under side of gaff close to the mast.

C—Measured from fore side of mainmast (in a line with main boom gooseneck) to gaff close in to mast.

D—Perpendicular to C, measured in to mast in a line with top of fore boom or tack cringle.

#### BAT WING SAILS.

To find the area of bat wing sails: Multiply A by a1 and B by b1 and add the two products together and divide by 2, and multiply C by D and add the product to the other.

#### LUGSAILS.

In the case of a lugsail, standing lugsail or balance lugsail being carried, the actual area of the same shall be computed; and if headsail be also carried, the measurements for computing the area of the same shall be taken from fore side mast, etc., in accordance with the method provided in the rule for headsails.

#### ROUND IN SAIL.

To compute the area bound by the round in the head of a lugsail or the foot of a loose-footed mainsail, when extending below the boom (or leach luff, etc., if extended by battens), multiply the base E by two-thirds of the perpendicular P (see diagram).

#### MODE OF MEASURING.

In cases of disputed measurements, or if the necessary measurements cannot be obtained from the designer or sailmaker, the sail can be measured in the manner following: Take the length of boom from mast to end and length of gaff from mast to end (any extension of gaff or boom to be considered part of gaff or boom, and to be extended when measured, to its full limit), then hoist the sail with the tack fast and set the luff and peak up taut, and let go the topping lifts so that the weight of the boom comes on the leach of the sail. With a line and tape measure the leach and luff and the diagonal C. For the headsail measure the height I and the distance J, as provided for in the section dealing with headsail. For topsail the sail would be hoisted and marked in a line with the gaff, then lowered and the other dimensions taken. From the measurements so taken a sail plan would be made and the areas calculated as described.

All measurements to be taken when the sails are perfectly dry and properly set.

#### SAILS OF UNUSUAL FORM.

If the various methods of measuring sails as herein shown do not, in the opinion of the Measurer, cover the case, he is to divide the sails into such triangles or figures as will get at accurate results, and a sail plan showing the manner in which the yacht has been measured, giving the different measurements and the points from which they have been taken, shall be furnished by the Measurer to the owner of every yacht measured by him.

#### TOTAL AREA.

In sloops and cutters the total sail area for purposes of measurement shall be the sum of the area of mainsail and topsail and headsails, as calculated by the method previously described; in yawls the sum of the areas of mainsail and topsail, mizzen and topsail and headsails; in schooners, the sum of the areas of mainsail, and topsail foresail and topsail and headsails; in three-masted schooners, the sum of the areas of the mizzen, mainsail, foresail and topsails and headsails.

A spinnaker may have a headstick, or board not longer than one-twentieth the length of the spinnaker boom, but not a foot-yard, or more than one sheet, or any other contrivance for extending the sail to other than a triangular shape.

In case a yacht shall carry a square-sail, or square topsail, or raffe (together or separately) instead of a spinnaker, the actual area of the same shall be computed; and if such area exceed the area of the fore-triangle, the excess shall be used in the total area for determining the rating.

#### Displacement.

D—May be obtained by any one of the following methods:

1.—By weighing the yacht when fully equipped and in racing trim.

2.—By acceptance of figures of designer.

3.—At points dividing the length of the load water-line into five equal parts, find areas of immersed cross-sections in square feet (recommended by fitting templates against side of yacht); from the areas in square feet obtained and load water-line length, find approximate displacement in cubic feet, which will be the D in formula.

#### Limits and Penalties.

One-half ( $\frac{1}{2}$ ) of any excess of L.W.L. over one hundred and fifteen (115) per cent of L, shall be added to the rating measurement.

The L. W. L. shall be the distance in a straight line between the points farthest forward and farthest aft, where the hull, exclusive of the rudder post is intersected by the surface of the water when the yacht is afloat in racing trim.

The limit of draft of yachts shall be in feet: 0.15 times the L, plus 2.50, and any excess of draft exclusive of center-board, as per above formula, shall be multiplied by three and added to the Rating Measurement; this penalty, however, shall not apply to yachts launched prior to November 1st, 1906, provided that such yachts do not increase their draft.

Any excess of the square root of the sail area over 135 per cent of L shall be added to the Rating Measurement; this penalty, however, shall not apply to yachts launched prior to November 1st, 1906, provided that such yachts do not increase their sail area.

Hollow spars shall not be allowed on any yacht except on those launched prior to November 1st, 1906.

#### TRIM.

All measurements of hull shall be taken with only such persons on board as shall be required to assist the Measurer.

#### Certificates of Measurement.

The Measurer may accept drawings, dimensions, and calculations of any or all specified measurements when certified to by the designer; but previous



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Arnold & Co., Ogdensburg, N. Y., India Pale Ale

Malt Marrow, McAvoy, Chicago

Sheboygan Mineral Water Co., Sheboygan, Wis.

Berghoff Brewing Co., Ft. Wayne, Ind., Dortmunder Beer

Piel Bros., Brooklyn, N. Y., Beer

John Meyer, Mt. Clemens, Lithiated Mt. Clemens Water

to the filing of a certificate of measurement with the Regatta Committee, the Measurer shall personally measure the spars and verify the line of flotation, and all dimensions and calculations depending thereon.

Should any designer wilfully issue a false certificate, all future yachts built from his designs after the date of such discovery, will be excluded from racing and future certificates of such Designer shall not be accepted.

If from any peculiarity in the build of a yacht, or other cause, the Measurer shall be of the opinion that the rule will not rate the yacht fairly, or that in any respect she does not comply with the requirements of these rules, he shall report the circumstances to the Regatta Committee, who, with the Measurer, after due inquiry, shall award such certificate of rating as they may consider equitable, and the measurement shall be deemed incomplete until this has been done.

### RULE III.

#### Classification.

Yachts shall be classified by Rating Measurement and shall be divided into classes as below:

#### SCHOONERS.

64 Foot Class—D Not over 64 feet and over 55 feet rating measurement.

55 Foot Class—E Not over 55 feet and over 47 feet rating measurement.

47 Foot Class—DD Not over 47 feet and over 40 feet rating measurement.

40 Foot Class—EE Not over 40 feet rating measurement.

#### SLOOPS AND YAWLS.

68 Foot Class—J Not over 68 feet and over 57 feet rating measurement.

57 Foot Class—K Not over 57 feet and over 48 feet rating measurement.

48 Foot Class—L Not over 48 feet and over 40 feet rating measurement.

40 Foot Class—M Not over 40 feet and over 33 feet, rating measurement.

33 Foot Class—N Not over 33 feet and over 27 feet rating measurement.

27 Foot Class—P Not over 27 feet and over 22 feet rating measurement.

22 Foot Class—Q Not over 22 feet and over 18 feet rating measurement.

18 Foot Class—R Not over 18 feet and over 15 feet rating measurement.

15 Foot Class—S Not over 15 feet rating measurement.

Every yacht of forty (40) feet rating measurement and under, launched after November 1st, 1906, shall rate at the highest limit of the class.

The Regatta Committee, in the notice of a race, shall specify for what class or classes the race is given; and in such notice, may combine classes, and establish special classes and conditions therefor.

### RULE IV.

#### Mixed Races. Allowance for Rig.

In races where vessels of different rigs sail together a schooner shall be rated for time allowance at eighty-five (85) per cent of her rating measurement; a yawl at ninety-three (93) per cent, and a sloop at her actual rating measurement.

### RULE V.

#### Time Allowance.

Time allowance shall be calculated according to the table of allowance, as shown on pages 91 to 95 inclusive.

### RULE VI.

#### Restrictions. (Racing Trim.)

1. SAILS. A yacht in races may carry the following sails:

A Sloop, mainsail, gaff or club topsail, forestaysail, jib, flying-jib, jib-topsail, and spinnaker.

A Yawl, same as a Sloop, with mizzen, mizzen topsail and mizzen staysail.  
A Schooner, mainsail, foresail, forestaysail, jib, flying-jib, jib-topsail, fore and main gaff or club topsail, main-topmast-staysail, and spinnaker.

A Three-Masted Schooner, same as a Schooner, with mizzen, mizzen-top-sail, and mizzen-staysail.

A Yacht may set light sails over or in place of working sails, and may carry square sails, square topsails, and rafters (together or separately) in place of a spinnaker. A pole-masted yacht may carry her regular sprit topsails.

2. LIFE BUOYS. Each yacht shall carry at least two serviceable life buoys, or belts, on deck, ready for use.

3. BOATS. Each yacht of any class named below shall carry on deck a serviceable, non-collapsible boat with round bottom, of a length not less than herein specified for her class; such boat to have oars and oar-locks lashed in.

Class D of Schooners and J of Sloops and Yawls, twelve feet in length. Classes E, DD, and EE of Schooners and K and L of Sloops and Yawls, ten feet in length.

4. BULKHEADS, BALLAST, ETC. Floors must be left down and bulkheads and doors left standing; water tanks kept in place, and at least one bower anchor and suitable cable kept on board. Each yacht shall keep her galley fixtures and fittings on board and in their proper places. Trimming by dead weight shall not be allowed after the preparatory signal. Ballast shall not be taken in or discharged after 9 p. m. of the day before a race.

A race postponed or resailed shall, so far as regards this rule, be considered a new race.

5. CREW. The total number of men permitted on a yacht during a race shall not exceed that given by the following table:

#### SCHOONERS.

Classes D, E, and DD, one man for every 250 square feet of sail area and fraction thereof.

Class EE, 40 foot Class, 9 men.

#### SLOOPS AND YAWLS.

Classes J, K, and L, one man for every 250 square feet of sail area and fraction thereof.

Class M, 40-foot class, 9 men.

Class N, 33-foot class, 6 men.

Class P, 27-foot class, 5 men.

Class Q, 22-foot class, 4 men.

Class R, 18-foot class, 3 men.

Class S, 15-foot class, 3 men.

#### CORINTHIANISM.

Yachts of 33-foot rating measurement and under must be steered by a Corinthian who must be a member of a club belonging to an association, which association must be a member of the Union. Corinthianism to be defined as under:

"Corinthianism in yachting is that attribute which represents participation for sport as distinct from gain, and which also involves the acquirement of nautical experience through the love of sport rather than through necessity or the hope of gain. It is consistent with the motive higher than mercenary found in the ranks of officers of the navy and naval architects, notwithstanding the remuneration they receive, while it is inconsistent with the trade of the fisherman, even though one following such a trade has never been a paid sailor. In this respect the following general definition is given:

"No person who follows the sea as a means of livelihood, or who has accepted remuneration for services rendered in handling or serving on a yacht, or who is a professional in any other sport, shall be considered a Corinthian yachtsman."

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Lv. Put-in-Bay . . . . .	11:30 A. M.	Lv. Kelleys Isl. . . . .	4:15 P. M.
Lv. Middle Bass . . . . .	11:45 A. M.	Lv. Middle Bass . . . . .	4:45 P. M.
Lv. Kelleys Isl. . . . .	12:30 P. M.	Lv. Put-in-Bay . . . . .	5:00 P. M.
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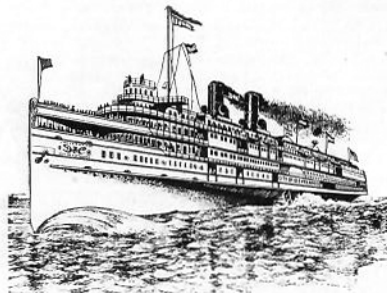
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The total number of professional sailors employed on a yacht during a race of the 33 and 27-foot classes shall not exceed two, and of the 22-foot class and under shall not exceed one.

6. **BOARDING AND LEAVING.** No person shall board or leave a yacht after the preparatory signal for her class has been made, except in case of accident, or injury to a person on board.

7. **PRIVATE SIGNAL AND NUMBER.** Each yacht shall carry her private signal at the main peak, and display her racing number above the reef points at approximately equal distances from the luff and leach on both sides of the mainsail.

8. **LIGHTS AND FOG SIGNALS.** The Government regulations regarding lights and fog signals shall be observed.

9. **PROPULSION.** No means of propulsion other than sails shall be employed.

10. **ANCHORING, ETC.** A yacht may anchor, but must weigh anchor again and not slip. A yacht shall not warp nor kedge, nor make fast to a buoy, pier, vessel or other object, except as provided in Section 12 of this Rule.

11. **SOUNDING.** No other means of sounding than the lead and line shall be employed.

12. **RUNNING AGROUND AND FOULING.** A yacht running aground or fouling a buoy, pier, vessel, or other object, may use her anchors, warps, boats, etc., to get clear, but may not receive any assistance, except from the crew of the vessel fouled.

13. **POWER.** Manual power only shall be used in handling sails.

#### **RULE VII.**

##### **Restrictions. (Cruising Trim.)**

When cruising trim is required a yacht shall conform to the following restrictions:

1. **FITTINGS.** Cruising deck, cabin, galley, and forecabin fittings and fixtures (davits excepted) shall be kept in place. Davits must be kept on board.

2. **ANCHORS AND CABLES.** Cruising equipment of anchors and cables suitable to the size of the yacht shall be carried, and of these one bower anchor with suitable cable bent shall be kept ready for immediate use.

3. **SAILS.** A yacht may carry the sails allowed in Racing Trim, except that topsails extending above the truck of the topmast or beyond the end of the gaff shall be barred. A pole-masted yacht may carry her regular sprit-topsails.

4. **BOATS.** Each Schooner over 55 feet and each Sloop or Yawl over 57 feet rating measurement shall carry two serviceable non-collapsible boats not less than 12 feet in length; each other Schooner and each Sloop or Yawl under 57 feet and over 40 feet rating shall carry one such boat not less than 10 feet in length; each Sloop or Yawl under 40 feet and over 22 feet rating shall carry or tow one such boat not less than 8 feet in length.

5. A yacht shall conform to all the restrictions of Rule VI. (Racing Trim) which are not replaced or modified by the foregoing sections of this Rule.

#### **RULE VIII.**

##### **Entries.**

1. A yacht cannot be entered for a race unless her ownership, rig, and measurement for time allowance, as computed in accordance with the Rules of the Union, are on record with the Regatta Committee.

2. Entries shall be made as required by the Regatta Committee in the notice of the race.

3. If two or more entries have been received for a class, a yacht starting alone in such class shall be entitled to sail over the course for a "Sail Over" prize; such yacht may, however, enter in any class above at the maximum

rating of such class, provided she notify the Regatta Committee and her competitors by hoisting at the starboard main spreader the letter designating the class she so enters. Such letter must be kept flying during the entire race.

4. Two or more yachts owned, wholly or in part, by the same person shall not be entered in the same class.

#### **RULE IX.**

##### **Instructions.**

1. The owner of each yacht entered for a race shall be furnished at the time of the entry, or as soon thereafter as possible, with written or printed instructions as to the conditions of the race and the courses to be sailed.

2. The Regatta Committee may change the courses or amend the instructions, provided notice of such change is given to each yacht before the warning signal is made.

#### **RULE X.**

##### **Amenable to Racing Rules.**

A yacht shall be amenable to the Racing Rules from the time the preparatory signal for her class is made.

From the setting of the warning signal, yachts whose preparatory signal has not been made, must keep clear of other yachts whose preparatory signal has been made. After crossing the finishing line a yacht shall not interfere with any yacht still in the race.

#### **RULE XI.**

##### **Start and Finish.**

1. The starting and finishing lines shall be indicated by a stake-boat or other mark at one end, and a white flag displayed on the Regatta Committee's boat or station at the other end.

2. The Regatta Committee may establish, by means of a buoy or buoys, a restricted area about the starting line, within which no yacht should enter until the preparatory signal of her class is made.

3. The Committee boat or station at the finish line shall show a red ball, and after sunset shall show two red lights.

A person may be placed on the stake-boat or station, at the finishing line, who shall time the yachts in the absence of the Regatta Committee.

4. If any yacht, or any part of her hull or spars, be on or across the line before the signal to start is given, she must return and recross the line; a yacht so returning, or one working into position from the wrong side of the line after the signal to start has been given, must keep clear of all competing yachts.

5. A yacht shall be considered to have finished a race, when, on completing the course, any part of her hull or spars shall be on or across the line.

#### **RULE XII.**

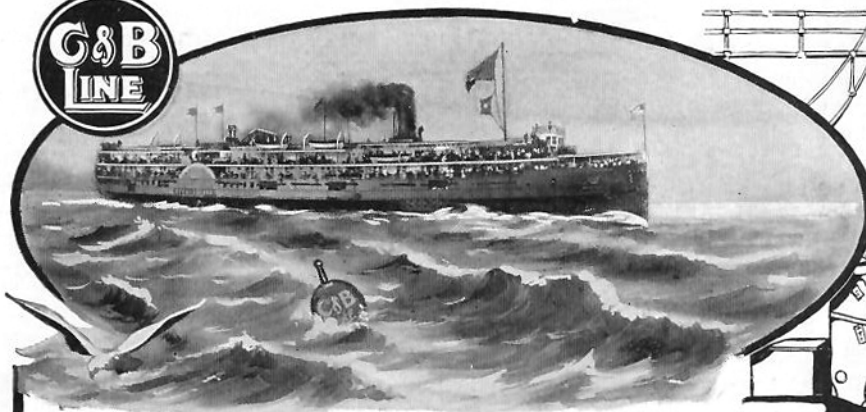
##### **Starting Signals.**

1. The Starting Signals for all races, to which attention shall be called by gun or whistle, shall be as follows:

**WARNING.** The hoisting of a white flag or cone.

**PREPARATORY.** Five minutes after the warning signal, the lowering of the white flag or cone and the hoisting of the blue peter or blue peter cone.

**START.** Five minutes after the preparatory signal, the lowering of the blue peter or blue peter cone and the hoisting of a distinctive signal, as provided in instructions.



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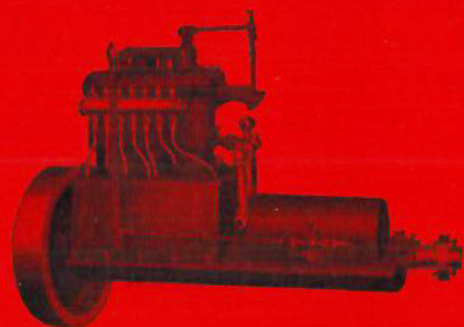
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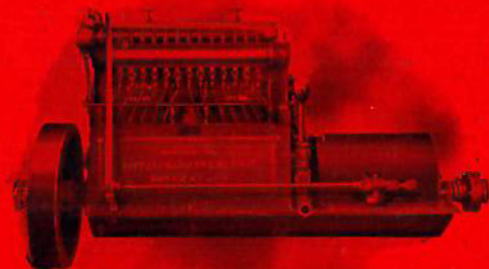
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2. In starting yachts by classes or divisions, the classes or divisions shall be started at five-minute intervals. The starting signal for one class or division shall be the preparatory signal for the next succeeding class or division.

3. The Regatta Committee may alter the time intervals between signals, when so stated in the instructions.

4. All starts shall be flying and shall be one gun starts unless otherwise specified in the instructions for the race.

5. When the time of starting is postponed until later in the day; postponements shall be for even fifteen minute intervals.

### RULE XIII.

#### Right of Way.

##### DEFINITIONS:

(a) Close-hauled.

A yacht is close-hauled when sailing by the wind as close as she can lay with advantage in working to windward.

(b) Wind Aft.

A yacht with the wind aft is deemed to have the wind on the side opposite to that on which she is carrying her main boom.

(c) Overlap.

An overlap is established when an overtaking yacht has no longer a free choice on which side she will pass, and continues to exist as long as the leeward yacht by luffing, or the weather yacht by bearing away, is in danger of fouling.

(d) Mark.

A mark is any vessel, boat, buoy, or other object used to indicate the course, and is not a mark until the starting signal has been made, and does not involve any question of sea-room.

##### RIGHT OF WAY.

When one yacht is approaching another yacht, so as to involve the risk of fouling, one of them shall keep clear of the other as follows:

###### 1. ON DIFFERENT POINTS OF SAILING.

A yacht free shall keep clear of one close-hauled.

###### 2. ON SAME POINT OF SAILING, WITH WIND ON OPPOSITE SIDES.

When both yachts are close-hauled, or both free, or both have the wind aft, and have the wind on opposite sides, the yacht with the wind on the port side shall keep clear.

###### 3. ON SAME POINT OF SAILING, WITH WIND ON SAME SIDE.

When both yachts are free, or both have the wind aft and have the wind on the same side, the yacht to windward shall keep clear.

###### 4. CONVERGING CLOSE-HAULED.

When two yachts, both close-hauled on the same tack, are converging by reason of the leeward yacht holding a better wind and neither can claim the rights of a yacht being overtaken, then the yacht to windward shall keep clear.

###### 5. ALTERING COURSE.

When, of two yachts, one is obliged to keep clear, the other shall not so alter her course as to involve the risk of fouling.

###### 6. OVERTAKING, LUFFING, AND BEARING AWAY.

A yacht overtaking another yacht shall keep clear of the overtaken yacht:

(a) Passing to Windward.

An overtaken yacht may luff as she pleases to prevent an overtaking yacht passing her to windward, until she is in such a position that her bowsprit end, or stem if she has no bowsprit, would strike the overtaking yacht abaft the main shrouds, when her right to prevent the other having a free passage to windward shall cease.

(b) Passing to Leeward.

An overtaken yacht must never bear away to prevent another yacht from passing her to leeward—the lee side to be considered that on which the leading yacht of the two carries her main boom. The overtaking vessel must not luff until she has drawn clear ahead of the yacht which she has overtaken.

##### 7. RIGHTS OF NEW COURSE.

A yacht shall not become entitled to her rights on a new course until she has filled away.

##### 8. PASSING AND ROUNDING MARKS.

If an overlap exists between two yachts when both of them, without tacking, are about to pass a mark on a required side, then the outside yacht must give the inside yacht room to pass clear of the mark.

A yacht shall not, however, be justified in attempting to establish an overlap and thus force a passage between another yacht and the mark after the latter yacht has altered her helm for the purpose of rounding.

##### 9. OBSTRUCTION TO SEA-ROOM.

When a yacht is approaching a shore, shoal, pier, rock, vessel, or other dangerous obstruction, and cannot go clear by altering her course without fouling another yacht, then the latter shall, on being hailed by the former, at once give room; and, in case one yacht is forced to tack or to bear away in order to give room, the other shall also tack, or bear away as the case may be at as near the same time as is possible without danger of fouling. But should such obstruction be a designated mark of the course, a yacht forcing another to tack under the provisions of this section shall be disqualified. (See Rule XX., Section 2, where this rule is particularly referred to.)

### RULE XIV.

#### Wrecking or Shifting of a Mark.

Should any mark be missing or moved from its proper position during a race, the Regatta Committee shall, if possible, replace it or substitute the Committee boat, and call attention by gun or whistle. Failing thus to re-establish the mark, the race may be ordered resailed or not, at the option of the Regatta Committee.

### RULE XV.

#### Accidents.

Each yacht shall render every possible assistance to any vessel or person in peril, and if in the judgment of the Regatta Committee she shall have thereby injured her chances of winning, they shall order the race resailed between such yacht and the winner in her class.

### RULE XVI.

#### Postponed and Resailed Races.

1. At any time before the Preparatory Signal the Regatta Committee shall have power to postpone any race should unfavorable weather conditions render such postponement advisable. Should the race be postponed for the day, then such postponed race shall be considered a new race.



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2. At any time after the starting signal and before the finish the Regatta Committee shall have power to declare off or order resailed any race, should unfavorable weather conditions render a finish improbable within the time limit. New entries shall not be received for such resailed race, and a yacht disqualified in the original race shall not be eligible to start in the resailed race.

#### **RULE XVII.**

##### **Shortening Course.**

Should it be necessary during a race to shorten the course two blasts of the whistle or two guns fired shall show that the race is to be finished with the round about to be completed or at the mark where the Regatta Committee's boat is stationed when giving the signal, and the time allowance shall be proportionately reduced.

#### **RULE XVIII.**

##### **Time of Race Limited.**

Except when otherwise specified in the instructions, a race in any class in which no yacht has finished at one-half hour after sunset shall be declared off.

#### **RULE XIX.**

##### **Protest.**

A yacht having cause, during a race, to protest against any other yacht for violation of these rules shall at once display a flag in the rigging, which shall be known as the Protest Flag, and keep such flag flying during the entire race. A protest must be supplemented by a written statement of the facts, which must be sent to the Regatta Committee before 6 p. m. of the next (week) day following the race.

2. If through protest the measurement of any yacht be called in question, the Regatta Committee shall direct the Measurer by whom such yacht was measured, or if, in their judgment such Measurer is not available, then another Measurer, to remeasure the same, disregarding designers' drawings and certificates, and the result, as reported by him, shall be final. The expenses of such remeasurement shall be borne by the party against whom such protest is decided.

#### **RULE XX.**

##### **Disqualifications.**

1. Each yacht must go fairly around the course. A yacht touching a mark, unless wrongfully compelled to do so by another yacht, shall at once abandon the race.

2. If a yacht, in consequence of her neglect of any of these rules, shall foul another yacht, or compel another yacht to foul any yacht, mark or obstruction, or to run aground, she shall be disqualified and shall pay all damages; and any yacht which shall wrongfully cause another to luff, bear away, or tack, in order to avoid fouling, or shall without due cause compel another yacht to give room or to tack under Section 9, Rule XIII., or shall herself fail to tack or to bear away, as required in that section, or shall in any other way infringe or fail to comply with any of these Rules or attempt to win a race by other means than fair sailing and superior speed and skill, shall be disqualified.

3. After having been officially measured, a yacht shall not make changes in her trim affecting "L" or "D" nor make alterations in her sail plan, without written notice to the Regatta Committee, specifying the changes that have been made and embodying a request for remeasurement.

4. The Regatta Committee shall without a protest disqualify any yacht, should they know prior to the conclusion of the race that she has committed a breach of these rules. They may disqualify any yacht for a breach of these rules should such breach be brought to their attention in writing, prior to 6 p. m. of the (week) day following the race.

#### **RULE XXI.**

##### **Award of Prizes.**

1. The owner of a winning yacht, or his representative on the yacht during the race, shall, before the awarding of prizes, file with the Regatta Committee a declaration that all the rules and regulations were complied with.

2. A prize shall be awarded to that yacht in each class which makes the best corrected time over the course.

3. If a winning yacht be disqualified the prize shall be awarded to the yacht making the next best corrected time over the course.

## **Restrictions of the 21-foot Cabin Class**

1. A 21-foot Cabin Class Yacht is intended to be a seaworthy type of cruising and racing sloop or cutter, substantially constructed, properly ballasted, and with moderate sail-plan, either with flush deck or cabin trunk, having suitable cabin accommodations below, and conforming to the limitations herein mentioned. Yachts in this class are intended to be of ordinary type, and any evasion in the shape of Sharpie, Catamaran, double hull, or other unusual type, or any yacht fitted with bilge fins, bilge boards or other similar contrivances will not be countenanced.

2. The load-water-line length shall be the distance in a straight line between the points furthest forward and furthest aft where the hull, exclusive of the rudder stock, is intersected by the surface of the water when the yacht is afloat in racing trim in smooth water, and shall not exceed 21 feet.

All yachts shall be measured for load-water-line length, beam and freeboard with required ballast on board and in proper position, and all extra sails, spars and required equipment shall be on board and placed amidship. There shall also be on board at time of measurement a deadweight of 450, 600 or 750 pounds, according to whether the owner elects to carry a crew of three, four or five men, which dead weight shall also be placed amidships. The term "amidship," as here used, means the point midway on the load-water-line, and the requirement as to position of equipment applies only to time measurement is being taken.

If any portion of the stem, stern-post or other part of the yacht below the load-water-line projects beyond the length thus measured, such projection shall be added to the measured length, and a form resulting from the cutting away of the fair line of the stem, stern-post or the ridge of the counter for the apparent purpose of shortening the load-water-line shall be measured between fair lines.

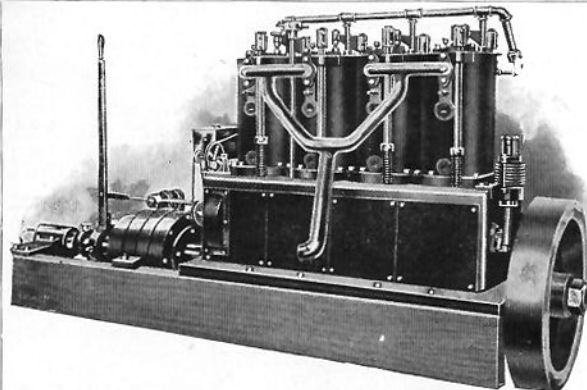
The Measurer, at the time of taking measurements, shall affix a distinctive permanent mark at each end of the load-water-line.

No adjustable or movable truss or other device for shortening or changing the load-water-line length of yachts shall be allowed.

A yacht altering her trim so as to increase her load-water-line length must immediately notify the Secretary of the Club or Association governing the race and obtain a new measurement.

3. The overall length shall be the distance in a straight line between the points of the hull furthest forward and furthest aft, and shall not exceed 36 feet.

4. A square or snub-nose bow shall not be allowed. The beam, measured on deck at a point equi-distant from the water-line forward and the extreme



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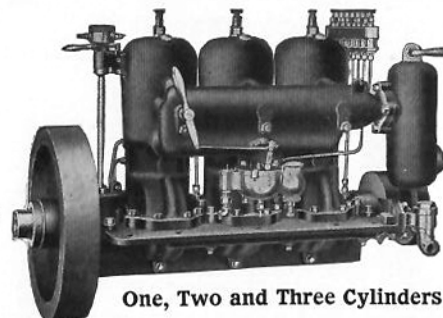
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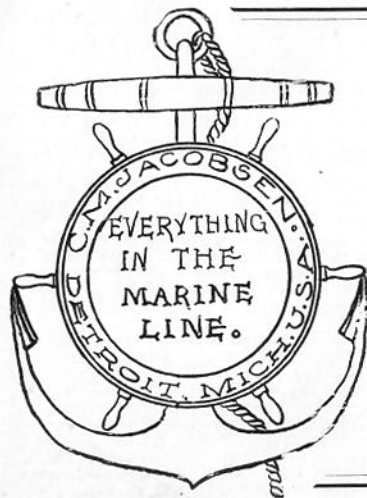
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bow, shall not exceed 45 per cent of the greatest water-line beam, and the deck line shall not run at an angle with the center-line greater than 30 degrees. Any evasion of the spirit as well as the letter of this rule shall disqualify a yacht from racing in this class.

The length of the forward or after overhang shall not exceed 65 per cent of the total overhang.

#### BEAM.

5. In computing the allowances specified in tables, the beam shall be measured at L. W. L.

#### COCKPIT.

6. The cockpit floor shall be above the L. W. L. with scuppers draining out-board.

#### FREEBOARD.

7. The least freeboard shall be measured from the top of covering board to the surface of the water.

Beam of, or less, than 7 feet.

FREEBOARD (minimum), 20 inches. Less one (1) inch for every foot of additional L. W. L. beam.

8. \*Any excess of freeboard over the minimum required, shall be deducted from the height allowed for the cabin trunk.

#### CABIN TRUNK.

9. The height of the cabin trunk measured from the level of top side of covering board to highest point of top side of cabin trunk, shall not exceed two (2) inches for every foot of greatest beam. (See Section 8.) The width of cabin trunk shall be not less than 60 per cent of the greatest beam.

10. †If the required head-room is obtainable under this rule, additional height of cabin trunk is allowed.

#### CABIN FLOOR.

11. The minimum width of the cabin floor space above the cabin floor shall be not less than one-quarter of the greatest beam.

CABIN FLOOR, length (minimum), 7 feet.

To be measured a distance below the cabin trunk beams equal to the required head-room.

#### HEAD-ROOM.

12. The minimum head-room under deck or cabin trunk beams shall be in the clear over the entire required cabin floor space above the cabin floor, exclusive of skylights and hatches.

HEAD-ROOM (minimum), 3 feet 10 inches.

\* "That the excess of freeboard deducted under Section 7 from the height allowed for cabin trunk under Section 9 shall be measured at the after end of the cabin floor space over which the specified head-room is required, and that the allowed height of cabin trunk above the top side of covering board thus determined shall not be exceeded at any cross-section (except as is provided in Section 8)."

†Has no reference to the height of side of the cabin trunk, but refers to the highest point of the upper side of the top of the cabin trunk.

‡If additional height of cabin trunk is taken, an equal amount of additional head-room is required.

A reduction of  $\frac{1}{8}$  inch allowed on planking if canvas covered.

### SCANTLINGS.

#### 13. Dimensions and areas expressed in inches:

A—STEM, sided at Head.....	3
B—STERNPOST, sided at Tuck.....	3 ¼
C—KEEL, Minimum Thickness.....	3
Sectional Area .....	48
D—FRAMES, Sectional Area—Heels.....	2 ¾
Bilge .....	2 ¼
Heads .....	1 ¼
Spacing (Maximum) .....	10
E—FLOORS, Sectional Area .....	5
Spacing (Maximum) .....	18
F—SHELF or CLAMP, Sectional Area—Middle.....	4 ½
Ends .....	3
G—BILGE STRINGERS, Sectional Area—Middle.....	3
Ends .....	2
H—DECK BEAMS, Sectional Area—Main.....	4 ½
Auxiliary .....	2 ¾
Half-Beams .....	1 ¾
Spacing (Maximum) .....	10
I—PLANKING, to finish full.....	¾
Hoodends (above L. W. L.).....	¾
J—*DECK and Cabin Trunk Top to finish full.....	¾

\* A reduction of  $\frac{1}{8}$  inch allowed on planking if canvas covered.

#### GENERAL SPECIFICATIONS AND EXPLANATIONS OF SCANTLING TABLE.

A—STEM. The minimum siding (thickness) measured at the rabbet at highest point on stemhead, no decrease of siding allowed.

B—STERNPOST. Minimum siding at tuck (the crossing of the rabbet). The siding may diminish from tuck to heel. The rudder stock, if of wood, to be equal in diameter to the siding of post.

C—KEEL. Minimum depth allowed for a length of one-third of the L. W. L. length beyond which may be tapered to one-third less at stem and stern. The minimum of sectional area, including keelson and deadwood (breadth multiplied by depth in the middle of keel), may be made up, if desired, by a deeper keel. The breadth of keel will taper from point of greatest section to siding of stem and sternpost.

D—FRAMES. The sizes laid down in the table show the minimum sectional area of frames (the siding multiplied by the moulding) at three points—the heel of frame where it is boxed into the keel, the middle of frame above the flat of the floor, and turn of bilge, and the head, at plank-sheer. The sectional area is that of a single frame for a uniform spacing between centres not exceeding that given in the table. This required area may be made up of smaller frames spaced closer together, or larger frames farther apart; or of combinations of large and small frames with appropriate spacings. This minimum sectional area shall apply to a space of at least two-thirds of the L. W. L. length in the center of the vessel; forward and aft of this, the sectional area may be reduced 20 per cent.

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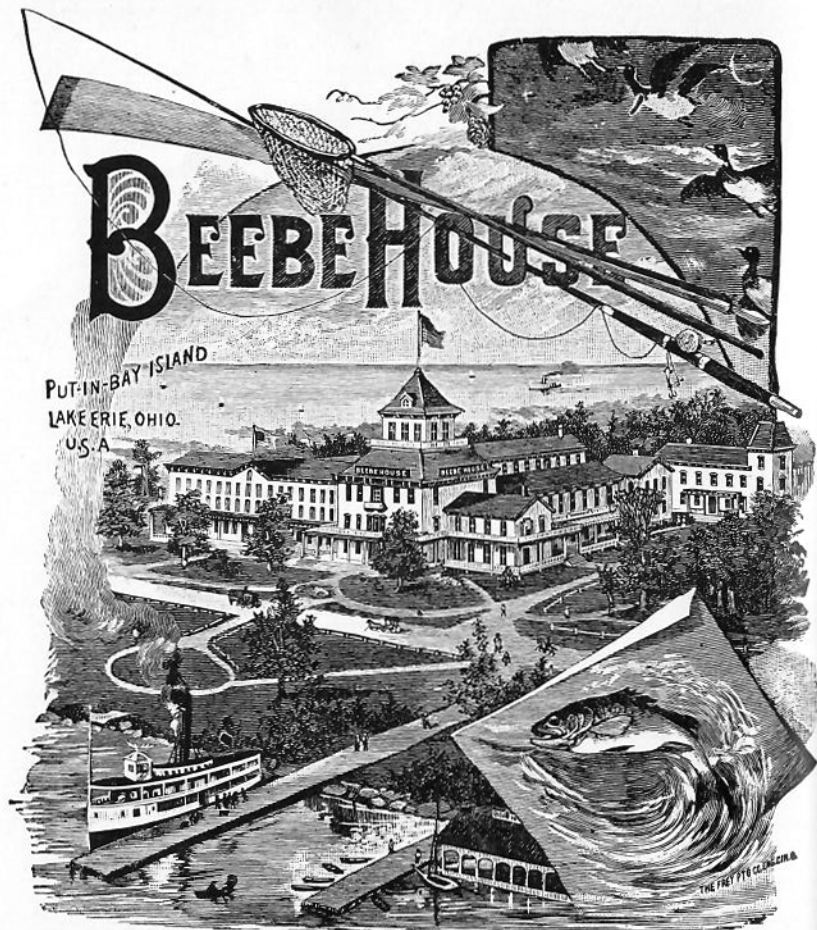
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Two adjoining frames abreast the mast, and one at each runner-plate must be increased in size in proportion as they are cut by the chainplate fastenings.

Where bent frames only are used, of one size throughout, they shall be not less than area required for bilge. Where bent frames are used in combination with sawn, the bent frames may be of uniform scantling from end to end; but the sawn frames must be large enough to make up the required average sectional area at the heels where they are cut by the fastenings of floors.

**SPACING OF FRAMES.** The maximum spacing of frames as given in the table is based not on the size of frames, this being variable, but on the thickness of planking allowed for the class; being the greatest spacing that will insure a tight seam with the usual caulking for the minimum thickness of planking allowed.

**E—FLOORS.** There shall be at least six strong floors in the center of the vessel in way of metal keel, and two at each mast step.

**F—SHELF OR CLAMP.** The minimum sectional area given for the middle shall cover a length of at least one-half of the shelf (or clamp) and in the middle, a taper being allowed to the size given at each end. The ends of deck beams may be jogged into top of shelf a distance not exceeding one-third of their own depth. If a beam clamp is used, fitted close up to the plank-sheer, the beams being thus jogged in for their full depth, the sectional area shall be increased in proportion.

**G—BILGE STRINGER.** The minimum sectional area at middle shall cover at least one-half the full length of bilge stringer, with taper allowed at the ends. At least one bilge stringer must be run on each side, at about the lower part of turn of bilge. In yachts whose extreme beam exceeds twice the greatest depth from underside of deck to upper side of keel, two such stringers on each side must always be fitted.

**H—DECK BEAMS.** The minimum sectional area of deck beams shall cover at least the middle third of the beam, allowing a taper, in the moulding, to each end. There must be one main beam at the bitts, two at the mast (partner beams), one at fore-end of cabin trunk, one at after end, two at each skylight, hatch and companion in flush-deck vessels, and one at transom. The auxiliary beams and the half beams abreast of house, skylights, etc., may be of the smaller areas given for each. The beams may be spaced at will, provided the maximum distance between centers does not exceed that given in the table which is based upon the thickness of deck planking. The beams should be jogged into the shelf or clamp a distance equal to one-third of the moulded depth of beams at end.

**I—PLANKING.** The dimensions given in the table are the minimum thicknesses allowed, after final planing, over a distance in the middle of the vessel equal to at least one-half of the overall length.

**J—DECKING.** The thickness given for the deck plank applies also to the cabin trunk top, plank-sheer (covering board), and the partner planks. The ends of the deck plank should be well supported, and in no case should they be wrought to a shim edge, which will crush down in caulking.

The keel, stem, sternpost, deadwood, frames, floors, cabin trunk beams and main deck beams shall be of oak, and solid.

**NOTE—**Builder's Certificate of scantling required for new yachts.

#### **SAIL AREA.**

14. The sail area, based on maximum class length and actual L. W. L. beam, shall not exceed the number of square feet specified in accompanying table and shall be measured as follows:

15. The actual area of the sails allowed in Section 27, except balloon jib and spinnaker, shall be measured by the official measurer. The number

of square feet in each sail shall be stamped on each sail by the official Measurer in full round black figures not less than three inches high. This number shall be known as the official number of the sail and shall always be visible. Any yacht using a sail not bearing the official number shall be disqualified, any rules to the contrary notwithstanding.

16. In no case shall the area of the mainsail and main topsail exceed 80 per cent of the entire area allowed.

17. The official Measurer shall be provided with the correct sail plan of any boat to be measured and shall cause distinguishing marks to be placed on the spars as follows: On the mast at the tack and at the throat of the mainsail; on the boom at the clew of the mainsail and on the gaff at the peak of the mainsail. There shall be only one mark at each point. These marks shall be black bands not less than one inch wide painted around the spars in a manner satisfactory to the Measurer. The inner edges of the bands shall mark the limits to which the sails may stretch without exceeding the sail area allowed. No part of any sail shall extend beyond these marks or beyond the point limited for spinnaker and jib halyard blocks in Section 21.

18. No yacht when close hauled shall carry any jib other than the working jibs for which she has been measured.

19. The distance from the center of the mast to the outer end of the spinnaker boom when the latter is at right angle to the fore and aft center line of the yacht, multiplied by the height of the spinnaker halyard block above the deck, shall not exceed 550 per cent of the actual area of the working headsails.

20. The distance from the forward end of the bowsprit to the center of the mast multiplied by the height of the highest jib halyard block above the deck shall not exceed 300 per cent of the actual area of the working headsails.

21. Spinnakers and all headsails the actual area of which is not measured shall be triangular. No spinnaker shall extend above the spinnaker halyard block, or beyond the end of the spinnaker boom. No jib shall extend above the highest jib halyard block, or beyond the end of the bowsprit.

#### **BALLAST.**

22. The weight of ballast, based on maximum class length and actual L. W. L. beam, shall be not less than the number of pounds specified in accompanying table and must be below the floor of cabin or cockpit.

#### **FIXTURES.**

23. The cabin shall contain two permanent berths or transoms, extending fore and aft, one on each side, which shall be not less than 6 feet and 6 inches in length and not less than two feet in width.

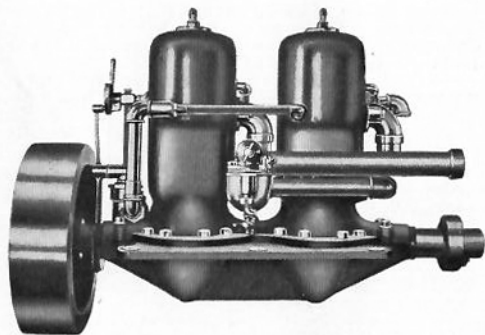
#### **FITTINGS.**

24. Each yacht sailing in races for this class shall have on board the following equipment: One anchor of not less than 35 lbs. in weight, a suitable anchor cable not less than 25 fathoms in length, a serviceable and suitable bilge pump, at least five life-preservers, a compass, a riding-light, a fog-horn, a bucket and a boat-hook.

#### **EXISTING YACHTS.**

25. Any yacht which existed on any one of the Great Lakes on November 14, 1903, which measures not to exceed 21 feet 6 inches on the load-water-line and conforms to these restrictions except as to overall and load-water-line length, may sail in this class.

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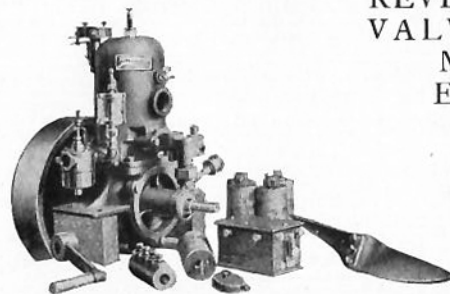
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**CREW.**

26. Yachts contesting in races for this class shall carry a crew of not less than three or more than five men. One member of a yacht's crew may be a professional; but the helmsman must be a Corinthian yachtsman and a member in good standing of some recognized yacht club on the Great Lakes.

**SAILS.**

27. Yachts in this class may carry the following sails: Mainsail, staysail, jib, jib-topsail and main-topsail, all of which shall be measured by the Measurer and included in the allotted area under the table in Section 28. Balloon-jibs and spinnakers may also be carried, but are not included in such allotted sail area.

28. Table of Maximum Sail and Minimum Ballast.

BEAM			BALLAST			BEAM			SAIL			BALLAST		
Ft.	In.	Sq. ft.	Pounds	Ft.	In.	Sq. ft.	Pounds	Ft.	In.	Sq. ft.	Pounds	Ft.	In.	Pounds
6		578	3616	9		830	1968							
	1	585	3573		1	837	1925							
	2	592	3530		2	844	1882							
	3	599	3487		3	851	1839							
	4	606	3444		4	858	1796							
	5	613	3401		5	865	1753							
	6	620	3358		6	872	1710							
	7	627	3315		7	879	1667							
	8	634	3272		8	886	1624							
	9	641	3129		9	893	1581							
	10	648	3086		10	900	1538							
	11	655	3043		11	907	1495							
7		662	3000	10		914	1452							
	1	669	2957		1	921	1409							
	2	676	2914		2	928	1366							
	3	683	2871		3	935	1323							
	4	690	2828		4	942	1280							
	5	697	2785		5	949	1237							
	6	704	2742											
	7	711	2699											
	8	718	2656											
	9	725	2613											
	10	732	2570											
	11	739	2527											
8		746	2484											
	1	753	2441											
	2	760	2398											
	3	767	2355											
	4	774	2312											
	5	781	2269											
	6	788	2226											
	7	795	2183											
	8	802	2140											
	9	809	2097											
	10	816	2054											
	11	823	2011											

**LIMITS**

## Rules and Restrictions of the 18-Foot Knockabout Class

Adopted December 29, 1906.

Amended December 14, 1907.

**DEFINITION.**

Yachts in this class shall be sea-worthy cabin boats and no catamaran, double-hull or other unusual type of boat, or one having square sides or square snub-nosed bow, or one fitted with bilge-boards, double rudders or similar contrivances shall be allowed.

Such boats shall have ballast on keel or fixed below floor and shall be rigged simply with mainsail, jib, balloon or reaching jib and spinnaker.

Any evasion of the spirit as well as the letter of these rules shall disqualify a boat from racing in this class.

**LENGTH.**

The length on the L. W. L. with full equipment on board shall not exceed 18 feet 6 inches or be less than 17 feet.

The overall length shall not exceed 31 feet and neither forward nor aft overhang shall exceed 65 per cent of the total overhang.

**BEAM.**

The beam at the L. W. L. in keel boats shall be at least 6 feet 1 inch and in center-board boats at least 6 feet 6 inches. The extreme beam at a point half way between the forward end of the L. W. L. and the extreme bow shall not exceed 40 per cent of the greatest W. L. beam. The girth of hull at said point shall not exceed the number of inches represented by the sum of beam, plus the depth of the hull, plus 3, measured at the same point. The girth, depth and beam shall be measured to a point 22 inches above the L. W. L. The freeboard at this point shall be not less than 22 inches.

**FREEBOARD.**

The minimum freeboard shall be at least 17 inches. The depth of hull, the girth and the freeboard shall be measured from the top-side of the covering board.

**DISPLACEMENT AND DRAFT.**

For centerboard boats the draft shall be not less than 2 feet 6 inches for at least four feet length of keel.

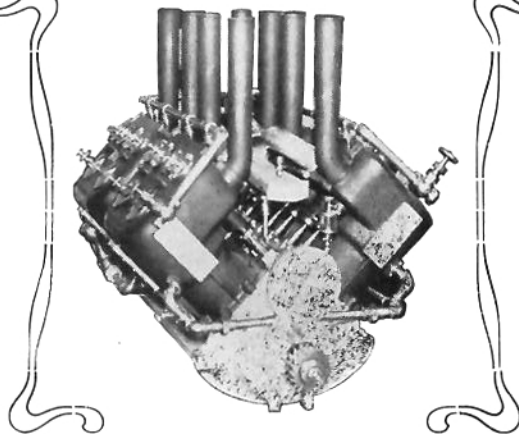
All boats shall weigh when rigged and equipped in accordance with these rules, not less than 4,000 lbs, exclusive of inside ballast. Not more than 50 per cent of the weight of a boat shall be in ballast. It shall be the duty of the Measurer to see that nothing is on board when the boat is weighed, except what these rules prescribe and that the boat is in every way in her normal condition.

**CABIN AND COCKPIT.**


The cabin house shall have a length of at least 6 feet and a width equal to 60 per cent of the greatest beam on deck; the sides and ends shall be vertical, have a height at the forward end of at least 4 inches and at the after sides of at least 6 inches above the deck. The top, sides and ends of cabin trunk must be permanently constructed and closed in with wood (port lights

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must be watertight) and with a companionway and door; the sill of the latter to be at least 3 inches above the cockpit floor. The cockpit floor shall be watertight and above the L. W. L. with scuppers draining outward and its sides and ends shall be bulkheaded and watertight.

No metal centerboard over 5-16 inch thick shall be allowed and wooden boards shall not be ballasted more than enough to overcome flotation.

#### SAILS.

The sail area shall not be over 450 square feet and not over 360 square feet of actual area shall be in the mainsail. The Measurer shall have a correct sail plan of any boat to be measured and shall cause distinguishing marks to be placed on the spars, as follows:

On at the mast at the tack and at the throat of the mainsail, on the boom at the clew of the mainsail, on at the gaff at the peak of the mainsail. No part of the mainsail shall be allowed to extend beyond these marks. The marks shall be black bands painted around the spars. The inner edge of the bands shall be the limit of the sail.

The actual area of the jib shall be measured.

The spinnaker shall be triangular. The spinnaker boom shall not exceed 12 feet in length from mast to end, and this rule shall apply to all new and existing boats. No battens over 26 inches in length to be used in sails. Spars shall be solid and round and sails made of cotton only. Amendment December 14, 1907, permitting yachts to carry hollow spars.

#### EQUIPMENT.

Equipment to include anchor of not less than 35 lbs., not less than 150 feet of  $1\frac{3}{8}$  inch rope; also bucket, pump, compass, foghorn, boat hook, lantern and four life preservers.

#### CREW.

Crew is limited to four persons and they shall all be Corinthians. The helmsman shall be a member of some organized Yacht Club.

#### CONDITIONS.

There shall be no time allowance between boats of this class. No boat

shall be painted or hauled out oftener than once a month, except in order to repair accidental damage.

#### EXISTING YACHTS.

Yachts built under and conforming to the restrictions of 1904 and yachts built under and conforming to the restrictions of the 18-foot knockabout class of the Yacht Racing Association of Massachusetts up to October 1, 1906, if no changes have been made, shall be admitted to this class, provided that, in the case of open boats, they shall be equipped with permanent cabin trunk at least 5 feet long, constructed substantially according to these rules for height and scantlings and conforming to the cockpit requirements.

#### TABLE OF SCANTLING

Stem, oak, sided at head inches.....	2½	Inches
Sternpost, oak, sided at tuck.....	2½	"
Keel, oak or rock elm, minimum thickness.....	2	"
Sectional area, square inches.....	10	"
Frames, oak or rock elm, sectional area, heels.....	1¼	"
Frames, oak or rock elm, sectional area, bilge.....	1	"
Frames, oak or rock elm, sectional area, heads.....	¾	"
Spacing .....	9	"
Floors, wood, sectional area sq. in.....	3	"
Shelf or clamps, oak, rock elm or hard pine.....	2½	"
Sectional area, square inch, middle.....	2½	"
Sectional area, square inch, ends .....	1½	"
Bilgestringer, oak, rock elm or hard pine, sectional area, square inch, middle .....	2½	"
Deck, beams, main, sectional area, oak.....	2	"
Auxiliary .....	1	"
Half beams .....	7/8	"
Spacing .....	9	"
Planking to finish full, inches.....	¾	"
Hood ends above L. W. L. ....	¾	"
Deck to finish full .....	1 5/8	"
Keel Bolts, spaced 12 inches .....	1/2	"
Metal Centerboard, thick .....	5-16	"



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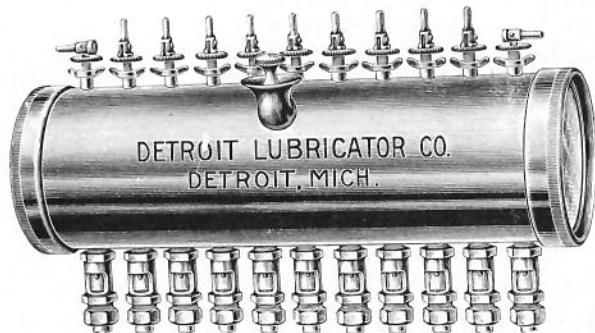
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# Rules and Restrictions for the 16-foot Class

Adopted December, 1904.

Amended December 2, 1905.

Amended December 14, 1907.

## I. Y. R. U. RULES.

Except as specified in these rules, the restrictions, scantling, scantling materials, ballast and equipment of all yachts shall be in accordance with the rules of the Yacht Racing Union of the Great Lakes.

## II. TYPE.

Yachts in these classes are intended to be of the ordinary type, and any evasion in the shape of catamaran, double hull or other unusual type, or any yacht fitted with bilge fins, bilge boards, double rudder or other similar contrivances, will not be accepted.

## III. BOW.

A square or snub-nosed bow shall not be allowed. The beam measured on deck at a point equi-distant from the water-line forward and the extreme bow shall not exceed 45 per cent of the greatest load water-line beam, and the deck line shall run at an angle with the center line not greater than 35 degrees. Any evasion of the spirit as well as the letter of this rule shall disqualify a yacht.

## IV. OVERHANG.

The total overhang shall not exceed 70 per cent of the class load water-line length, and neither the forward nor the aft overhang shall exceed 65 per cent of the total overhang.

## V. CABIN.

Cabin yachts shall have watertight self-bailing cockpits. For open yachts of the 16-ft. class, the cockpit shall have a maximum length of 8 feet, and a maximum width between coamings of 60 per cent of the extreme beam. The coamings shall have a minimum height of 3 inches. Open yachts shall be fitted with air tanks, or watertight bulkheads sufficient to float fixed ballast.

## VI. SAILS AND SPARS.

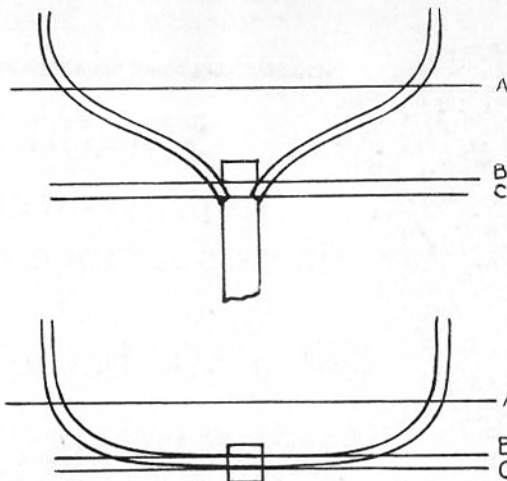
Yachts in these classes may be sloop or yawl rigged. For sloops the sails shall be restricted to mainsail, jib, spinnaker, and balloon jib; for yawls to mainsail, jigger, jib, spinnaker and balloon jib. The total area of the sails abaft the main mast shall not exceed 75 per cent of the total allowed sail area. Cotton sails shall be used. Yachts in this class shall have solid spars. Amendment December 14, 1907, permitting yachts to carry hollow spars.

The maximum length of the spinnaker boom shall be 75 per cent of the length of the main boom for sloops and 100 per cent of the main boom for yawls. The spinnaker halyard block shall not be above the jib halyard block.

## VII. MEASUREMENTS.

Measurements for load water-line length and freeboard shall be made with all racing equipment on board and without crew.

The immersed vertical cross-section shall be determined by measuring the area bounded by the outside of the hull and included between the load water-line plane and a line parallel to it and intersecting the keel one inch below the lowest point of the interior of the hull at the measured section.



A—Load water-line.

B—Line intersecting lowest point in interior of hull.

C—Line intersecting keel 1 inch below lowest point in interior of hull.

The owner of each yacht shall file with the Secretary of the Association a copy of the designer's draft of the largest immersed vertical cross-section, certified by designer and builder. The builder shall mark, by affixing round-headed brass screws on the outside of the hull, the upper and lower boundaries of the largest immersed vertical cross-section. Access to the bottom of the interior of the hull at the measured section shall be provided by hatch or otherwise, unobstructed by ballast, frames, floors or other timbers.

In computing the sail area the forward triangle, exclusive of the spinnaker boom, shall be measured.

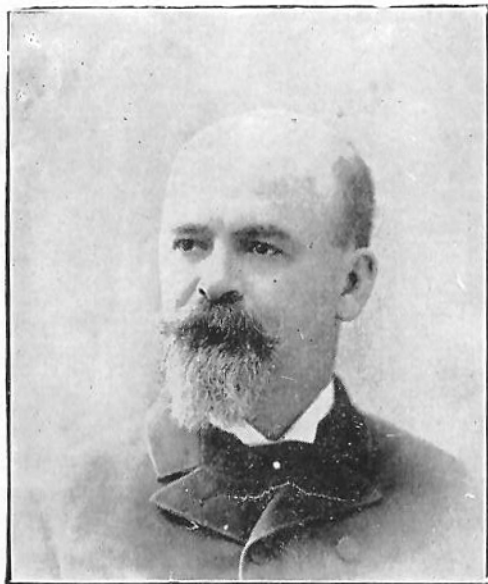
## VIII. CREWS.

The maximum number of persons in the crew of each boat shall be limited to three persons. Amendment adopted December 14, 1907. Crew is limited to three persons and they shall all be Corinthians. The helmsman shall be a member of some organized yacht club.

Yachts eligible to race under the rules of the 16-ft. class adopted in 1904, and yachts of the 15-ft. class of the Atlantic coast and the 16-ft. class of the Lake Sailing Skiff Association of Lake Ontario may race in the 16-ft. class.

## TABLE OF RESTRICTIONS.

	16-ft. class
Maximum load water-line .....	16 ft.
Minimum area of largest immersed vertical cross-section .....	4.50 sq. ft.
	K.
	16 in.
Minimum free board .....	C. B.
	15 in.
Maximum sail area, measuring fore triangle and excluding spinnaker .....	400 sq. ft.



Chew  
Beeman's Pepsin Gum

Cures Indigestion and  
Seasickness



**TABLE OF SCANTLING.**

Stem, oak, sided at head, inches	2¼ in.
Sternpost, oak, sided at tuck	2¼ in.
Keel, oak or rock elm, minimum thickness	1¾ in.
Sectional area, square inches	9 in.
Frames, oak or rock elm, sectional area, heels	1 in.
square inches	¾ in.
heads	¾ in.
Spacing	8 in.
Floors, wood, sectional area, sq. in.	2½ in.
Shelf or clamps, oak, rock elm or hard middle	2¼ in.
pine, sectional area, sq. in., ends	1½ in.
Bilgestringer, oak, rock elm, or hard pine, sectional area, sq. in.	
middle	2½ in.
Deck, beams, main, sectional area, oak	1 in.
Auxiliary	¾ in.
Half beams	¾ in.
Spacing	8 in.
Planking to finish full inches	5/8 in.
Hood ends above L. W. L.	½ in., "
Deck to finish full	¾ in.
Keel bolts, spaced 12 inches	¾ in.
Metal centerboard, thick	¼ in.

**IV. TABLE OF MINIMUM SCANTLING.**

Stem, oak, sided at head, inches	2¼ in.
Stern board, oak	1¼ in.
Keel, oak, sectional area	12 sq. in.
Frames, oak, sectional area, sharpie	2½ sq. in.
Frames, oak, sectional area, round bilge	1½ sq. in.
Frame spacing, centers	10 in.
Bilge stringer or chime piece, oak; sec. area of each	2 sq. in.
Floors, oak, sectional area	2 sq. in.
Deck, beams, oak, sectional area	1½ sq. in.
Deck beam spacing, centers	10 in.
Planking, to finish full	¾ in.
Decking, to finish full	¾ in.

**V. CREWS.**

The number of persons in the crew of each boat shall be two, who must be members of the entering club.

**Table of Time Allowance**

The allowances in this table are based upon the rule accepted by naval architects, that within economic limits opportunities for speed vary in different vessels as the square roots of their respective lengths. As strong winds are required, however, to give to larger vessels the full extent of their advantage in size, and as such a scale of allowance is not adapted to ordinary summer racing, 60 per cent only of the allowance due to the rule is given in the table, and may be stated thus:

Time equals  $.6 \left\{ \frac{3600}{\sqrt{L}} - \frac{3600}{\sqrt{l}} \right\}$ ; 3,600 representing the number of seconds in an hour, l the smaller yacht and L the larger one. Practically the formula is  $\frac{2160}{\sqrt{L}} - \frac{2160}{\sqrt{l}}$ ; 6-10ths of 3,600 being 2,160.

**RULE FOR USING THE TABLE.**

The figures to be found in the table show in seconds and hundredths of a second what a yacht of the measurement opposite to these figures would be allowed by one of 96.9 feet rating measurement in sailing one nautical mile. To find what a yacht of any measurement should receive from a larger one, take the figures to be found opposite to the smaller measurement; from these subtract the figures opposite to the measurement of the larger yacht, and the difference multiplied by the number of nautical miles in the course will give the amount of the allowance due to the smaller vessel, in seconds and hundredths of a second.

**EXAMPLE.**

What time will a yacht of 39 feet rating measurement have to allow to one of 36.7 feet rating measurement in a course of 20 nautical miles?  
 The time opposite 36.7 feet is.....167.11  
 The time opposite 39 feet is.....156.44

Difference ..... 10.67  
 Number of nautical miles..... 20

Allowance ..... 213.40  
 Or 3 minutes 33 2-5 seconds.

For the fractions of a foot in the measurement use the nearest tenth to be found in the table.

**Rules and Restrictions of the 15-foot Cat-boat Class**

Adopted December 29, 1906.

**I. TYPE.**

Boats in this class are intended to be of the ordinary type, and any invasion in the shape of catamaran, double hull, square or snub-nosed bow, or other unusual type, or any boat fitted with bilge fins, bilge boards, double rudders, or other similar contrivances, shall not be accepted. Rudders must be hung on a skeg or dead wood. No pot lead allowed.

**II. RATING MEASUREMENT.**

Boats shall not exceed 15 feet rating measurement, which shall be obtained as follows:

$$\frac{\text{Length Over All} + \text{Extreme Breadth}}{2} = \text{Rating Length.}$$

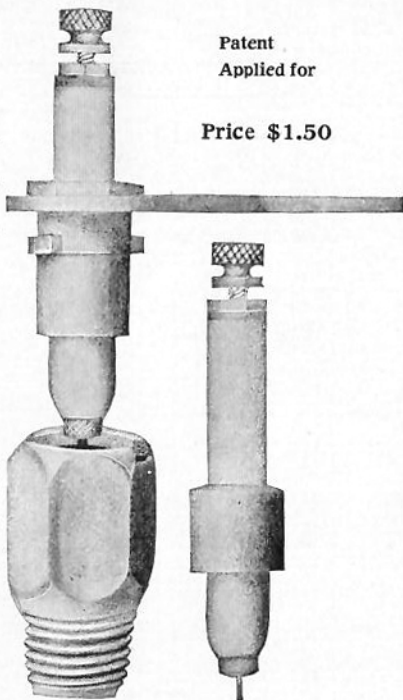
Provided that the length overall shall not exceed 22 feet, and the minimum free board shall be 14 inches.

**III. SAILS AND SPARS.**

Boats in this class shall be single cat rigged, and there shall be no other sails allowed. Cotton sails, manilla or cotton halyards and solid spars only shall be used. The area of the sail shall not exceed 18 square feet for each foot of Rating Length.

# The Sherman Spark Plug

A plug instantly separable for cleaning purposes, priming engine, or replacing the core practically soot proof, constructed to make it positively leak proof mechanically perfect and made of high grade material. For further information apply to



Patent  
Applied for

Price \$1.50

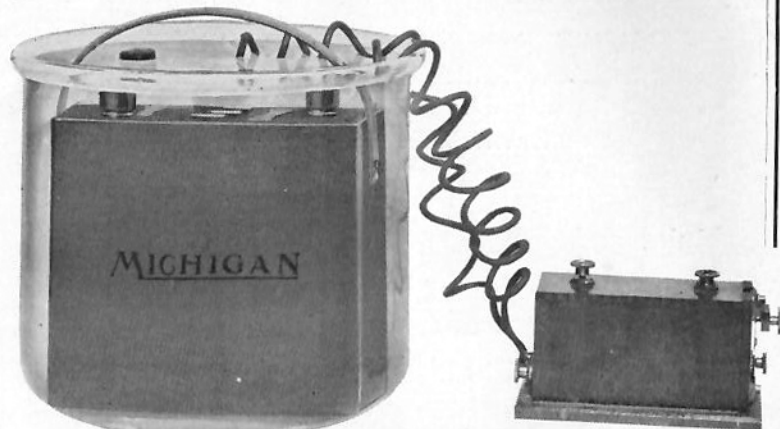
**G. H. Sherman & Son**

419 St. Aubin Ave.

Detroit, Mich.

# Michigan Storage Battery

Made in Detroit—Used Everywhere  
Water will not short circuit it



Greatest Mileage. Why? On account of the large amount of available current.  
The most Economical Battery. Why? On account of its solid construction; and does not require cleaning out every year.  
The most Efficient Battery. Why? On account of *having* the rated number of ampere hours.  
The ONLY Non-Corrosive Battery made. Why? Because no metal is used in its construction that can corrode. Always best in a competitive test.

## GUARANTEE

Our guarantee covers all material and workmanship, capacity and durability, for one year; but against abuse, carelessness or disregard for the proper care of a battery *we are not responsible*. We will gladly replace within one year any battery which does not come up to all claims we make for it.

Our success is due to the method we have pursued in doing business in the past, in which our motto has been "*A Satisfied Patron.*"

**Michigan Storage Battery Co.**

Detroit, Mich.

**TIME ALLOWANCE**

Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance
13.0	409.64	17.0	334.44	21.0	281.90	25.0	242.57
.1	407.35	.1	332.90	.1	280.79	.1	241.70
.2	405.08	.2	331.38	.2	279.68	.2	240.84
.3	402.84	.3	329.87	.3	278.57	.3	239.99
.4	400.63	.4	328.38	.4	277.48	.4	239.15
.5	398.44	.5	326.90	.5	276.40	.5	238.31
.6	396.27	.6	325.43	.6	275.32	.6	237.47
.7	394.13	.7	323.97	.7	274.25	.7	236.64
.8	392.01	.8	322.53	.8	273.18	.8	235.81
.9	389.92	.9	321.10	.9	272.12	.9	235.00
14.0	387.84	18.0	319.68	22.0	271.07	26.0	234.18
.1	385.79	.1	318.27	.1	270.02	.1	233.36
.2	383.76	.2	316.87	.2	268.99	.2	232.55
.3	381.76	.3	315.49	.3	267.96	.3	231.74
.4	379.77	.4	314.11	.4	266.94	.4	230.94
.5	377.80	.5	312.75	.5	265.92	.5	230.15
.6	375.86	.6	311.40	.6	264.91	.6	229.37
.7	373.93	.7	310.06	.7	263.92	.7	228.58
.8	372.03	.8	308.73	.8	262.92	.8	227.80
.9	370.14	.9	307.41	.9	261.94	.9	227.02
15.0	368.27	19.0	306.10	23.0	260.95	27.0	226.25
.1	366.42	.1	304.80	.1	259.98	.1	225.48
.2	364.59	.2	303.51	.2	259.01	.2	224.71
.3	362.78	.3	302.23	.3	258.05	.3	223.96
.4	360.98	.4	300.96	.4	257.09	.4	223.20
.5	359.20	.5	299.70	.5	256.14	.5	222.44
.6	357.44	.6	298.45	.6	255.19	.6	221.70
.7	355.70	.7	297.21	.7	254.26	.7	220.96
.8	353.97	.8	295.98	.8	253.32	.8	220.22
.9	352.26	.9	294.76	.9	252.40	.9	219.49
16.0	350.56	20.0	293.54	24.0	251.47	28.0	218.76
.1	348.88	.1	292.34	.1	250.56	.1	218.03
.2	347.22	.2	291.16	.2	249.65	.2	217.31
.3	345.57	.3	289.97	.3	248.74	.3	216.59
.4	343.93	.4	288.79	.4	247.84	.4	215.87
.5	342.31	.5	287.63	.5	246.95	.5	215.16
.6	340.71	.6	286.46	.6	246.06	.6	214.45
.7	339.12	.7	285.31	.7	245.18	.7	213.74
.8	337.55	.8	284.17	.8	244.31	.8	213.05
.9	335.99	.9	283.03	.9	243.43	.9	212.35

**TIME ALLOWANCE—Continued**

Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance
29.0	211.66	33.0	186.59	37.0	165.67	41.0	147.90
.1	210.97	.1	186.01	.1	165.19	.1	147.48
.2	210.29	.2	185.44	.2	164.71	.2	147.07
.3	209.60	.3	184.87	.3	164.22	.3	146.66
.4	208.92	.4	184.31	.4	163.7	.4	146.26
.5	208.25	.5	183.74	.5	163.2	.5	145.85
.6	207.58	.6	183.19	.6	162.8	.6	145.44
.7	206.91	.7	182.64	.7	162.5	.7	145.04
.8	206.24	.8	182.09	.8	161.88	.8	144.65
.9	205.58	.9	181.55	.9	161.42	.9	144.25
30.0	204.92	34.0	181.01	38.0	160.97	42.0	143.87
.1	204.26	.1	180.46	.1	160.50	.1	143.46
.2	203.62	.2	179.92	.2	160.04	.2	143.06
.3	202.97	.3	179.38	.3	159.59	.3	142.63
.4	202.32	.4	178.84	.4	159.13	.4	142.27
.5	201.67	.5	178.30	.5	158.68	.5	141.88
.6	201.04	.6	177.77	.6	158.22	.6	141.49
.7	200.40	.7	177.24	.7	157.76	.7	141.11
.8	199.78	.8	176.71	.8	157.32	.8	140.73
.9	199.15	.9	176.20	.9	156.88	.9	140.34
31.0	198.53	35.0	175.68	39.0	156.44	43.0	139.97
.1	197.90	.1	175.15	.1	156.00	.1	139.58
.2	197.28	.2	174.64	.2	155.56	.2	139.20
.3	196.66	.3	174.12	.3	155.11	.3	138.82
.4	196.03	.4	173.60	.4	154.67	.4	138.43
.5	195.42	.5	173.09	.5	154.26	.5	138.05
.6	194.81	.6	172.57	.6	153.80	.6	137.68
.7	194.20	.7	172.07	.7	153.37	.7	137.30
.8	193.60	.8	171.56	.8	152.94	.8	136.93
.9	193.00	.9	171.06	.9	152.52	.9	136.57
32.0	192.41	36.0	170.57	40.0	152.10	44.0	136.20
.1	191.81	.1	170.06	.1	151.67	.1	135.83
.2	191.21	.2	169.56	.2	151.24	.2	135.46
.3	190.62	.3	169.07	.3	150.80	.3	135.08
.4	190.03	.4	168.58	.4	150.38	.4	134.72
.5	189.44	.5	168.08	.5	149.96	.5	134.35
.6	188.86	.6	167.59	.6	149.54	.6	133.99
.7	188.28	.7	167.11	.7	149.14	.7	133.63
.8	187.72	.8	166.63	.8	148.72	.8	133.27
.9	187.15	.9	166.15	.9	148.31	.9	132.91

## TIME ALLOWANCE—Continued

Rating Measure- ment	Allow- ance	Rating Measure- ment	Allow- ance	Rating Measure- ment	Allow- ance	Rating Measure- ment	Allow- ance
45.0	132.56	49.0	119.15	53.0	107.27	57.0	96.67
.1	132.20	.1	118.82	.1	106.98	.1	96.41
.2	131.84	.2	118.50	.2	106.69	.2	96.16
.3	131.48	.3	118.19	.3	106.42	.3	95.90
.4	131.12	.4	117.88	.4	106.14	.4	95.65
.5	130.76	.5	117.56	.5	105.86	.5	95.40
.6	130.42	.6	117.25	.6	105.59	.6	95.16
.7	130.07	.7	116.95	.7	105.31	.7	94.92
.8	129.72	.8	116.64	.8	105.04	.8	94.68
.9	129.38	.9	116.34	.9	104.77	.9	94.44
46.0	129.05	50.0	116.04	54.0	104.51	58.0	94.20
.1	128.70	.1	115.73	.1	104.23	.1	93.95
.2	128.35	.2	115.42	.2	103.96	.2	93.70
.3	128.00	.3	115.12	.3	103.68	.3	93.44
.4	127.66	.4	114.82	.4	103.42	.4	93.20
.5	127.31	.5	114.50	.5	103.14	.5	92.96
.6	126.96	.6	114.20	.6	102.88	.6	92.72
.7	126.62	.7	113.90	.7	102.61	.7	92.48
.8	126.29	.8	113.60	.8	102.35	.8	92.24
.9	125.96	.9	113.32	.9	102.08	.9	92.02
47.0	125.64	51.0	113.03	55.0	101.82	59.0	91.79
.1	125.30	.1	112.73	.1	101.56	.1	91.54
.2	124.97	.2	112.43	.2	101.29	.2	91.30
.3	124.63	.3	112.13	.3	101.03	.3	91.06
.4	124.30	.4	111.84	.4	100.75	.4	90.82
.5	123.97	.5	111.54	.5	100.50	.5	90.58
.6	123.65	.6	111.25	.6	100.24	.6	90.34
.7	123.32	.7	110.96	.7	99.97	.7	90.10
.8	122.99	.8	110.80	.8	99.71	.8	89.88
.9	122.66	.9	110.51	.9	99.46	.9	89.65
48.0	122.34	52.0	110.11	56.0	99.20	60.0	89.42
.1	122.00	.1	109.81	.1	98.94	.1	89.18
.2	121.68	.2	109.52	.2	98.68	.2	88.94
.3	121.36	.3	109.24	.3	98.42	.3	88.72
.4	121.03	.4	108.95	.4	98.17	.4	88.49
.5	120.71	.5	108.66	.5	97.91	.5	88.26
.6	120.38	.6	108.37	.6	97.66	.6	88.03
.7	120.07	.7	108.10	.7	97.40	.7	87.80
.8	119.77	.8	107.82	.8	97.15	.8	87.58
.9	119.46	.9	106.54	.9	96.91	.9	87.35

## TIME ALLOWANCE—Continued

Rating Measure- ment	Allow- ance	Rating Measure- ment	Allow- ance	Rating Measure- ment	Allow- ance	Rating Measure- ment	Allow- ance
61.0	87.13	65.0	78.48	69.0	70.61	73.0	63.37
.1	86.90	.1	78.26	.1	70.42	.1	63.19
.2	86.68	.2	78.06	.2	70.22	.2	63.01
.3	86.45	.3	77.86	.3	70.03	.3	62.84
.4	86.22	.4	77.65	.4	69.84	.4	62.68
.5	85.99	.5	77.45	.5	69.65	.5	62.51
.6	85.76	.6	77.24	.6	69.47	.6	62.34
.7	85.54	.7	77.04	.7	69.29	.7	62.17
.8	85.32	.8	76.84	.8	69.11	.8	62.00
.9	84.10	.9	76.64	.9	68.93	.9	61.84
62.0	84.89	66.0	76.45	70.0	68.75	74.0	61.67
.1	84.66	.1	76.25	.1	68.56	.1	61.49
.2	84.43	.2	76.04	.2	68.36	.2	61.31
.3	84.20	.3	75.84	.3	68.17	.3	61.14
.4	83.99	.4	75.64	.4	67.99	.4	60.97
.5	83.77	.5	75.43	.5	67.81	.5	60.80
.6	83.56	.6	75.23	.6	67.63	.6	60.64
.7	83.34	.7	75.04	.7	67.45	.7	60.47
.8	83.12	.8	74.84	.8	67.27	.8	60.30
.9	82.91	.9	74.65	.9	67.09	.9	60.14
63.0	82.70	67.0	74.46	71.0	66.91	75.0	59.99
.1	82.48	.1	74.26	.1	66.72	.1	59.82
.2	82.26	.2	74.05	.2	66.54	.2	59.65
.3	82.04	.3	73.85	.3	66.36	.3	59.48
.4	81.83	.4	73.66	.4	66.18	.4	59.32
.5	81.61	.5	73.46	.5	66.00	.5	59.15
.6	81.41	.6	73.27	.6	65.82	.6	58.98
.7	81.19	.7	73.08	.7	65.64	.7	58.81
.8	80.98	.8	72.89	.8	65.46	.8	58.64
.9	80.77	.9	72.70	.9	65.29	.9	58.49
64.0	80.57	68.0	72.50	72.0	65.12	76.0	58.33
.1	80.35	.1	72.30	.1	64.94	.1	58.16
.2	80.14	.2	72.11	.2	64.76	.2	58.00
.3	79.92	.3	71.92	.3	64.58	.3	57.83
.4	79.72	.4	71.72	.4	64.40	.4	57.67
.5	79.51	.5	71.53	.5	64.24	.5	57.52
.6	79.30	.6	71.34	.6	64.06	.6	57.36
.7	79.09	.7	71.15	.7	63.88	.7	57.19
.8	78.89	.8	70.97	.8	63.71	.8	57.04
.9	78.68	.9	70.79	.9	63.54	.9	56.88

TIME ALLOWANCE—Continued

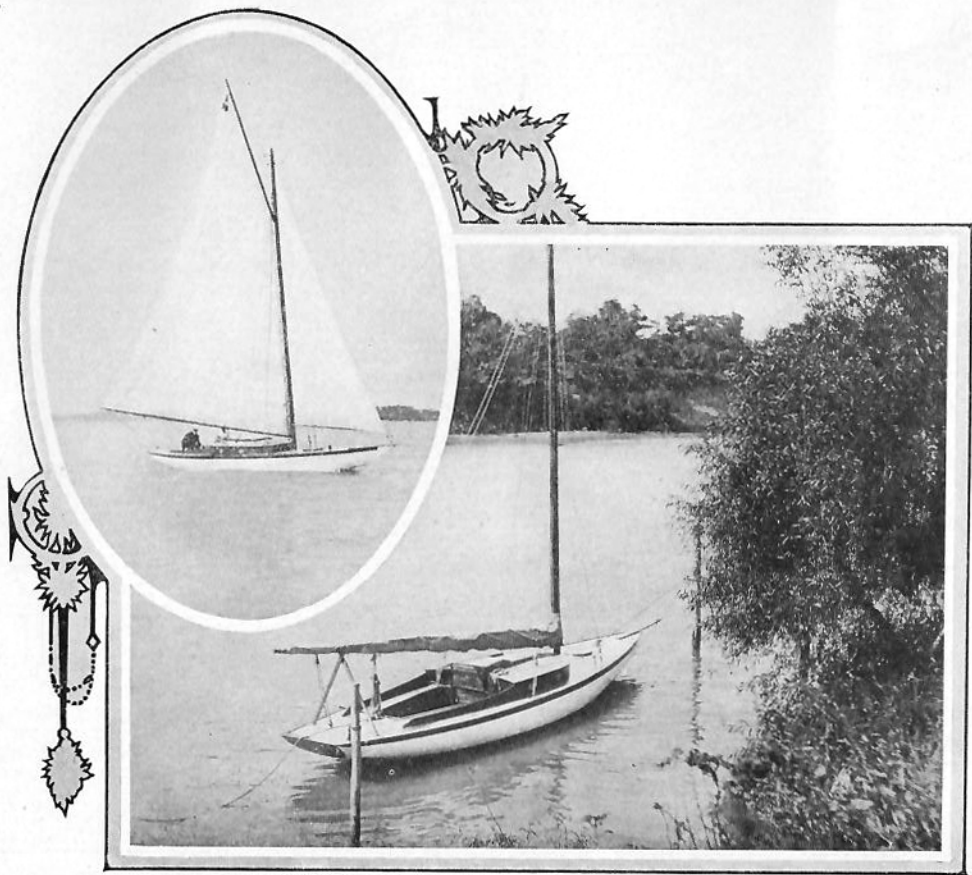
Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance
77.0	56.72	80.0	52.07	83.0	47.64	86.0	43.46
.1	56.56	.1	51.91	.1	47.50	.1	43.32
.2	56.39	.2	51.76	.2	47.35	.2	43.19
.3	56.23	.3	51.60	.3	47.21	.3	43.06
.4	56.08	.4	51.44	.4	47.06	.4	42.92
.5	55.92	.5	51.30	.5	46.92	.5	42.79
.6	55.76	.6	51.14	.6	46.78	.6	42.66
.7	55.61	.7	51.00	.7	46.63	.7	42.53
.8	55.45	.8	50.86	.8	46.49	.8	42.40
.9	55.30	.9	50.71	.9	46.36	.9	42.26
78.0	55.14	81.0	50.57	84.0	46.22	87.0	42.13
.1	54.97	.1	50.41	.1	46.08	.1	42.00
.2	54.82	.2	50.26	.2	45.94	.2	41.87
.3	54.66	.3	50.11	.3	45.79	.3	41.74
.4	54.50	.4	49.96	.4	45.65	.4	41.60
.5	54.35	.5	49.81	.5	45.50	.5	41.47
.6	54.19	.6	49.67	.6	45.36	.6	41.34
.7	54.04	.7	49.52	.7	45.23	.7	41.21
.8	53.88	.8	49.38	.8	45.10	.8	41.08
.9	53.74	.9	49.24	.9	44.96	.9	40.94
79.0	53.59	82.0	49.09	85.0	44.83	88.0	40.81
.1	53.44	.1	48.94	.1	44.69	.1	40.68
.2	53.28	.2	48.79	.2	44.54	.2	40.55
.3	53.12	.3	48.65	.3	44.40	.3	40.42
.4	52.97	.4	48.50	.4	44.26	.4	40.28
.5	52.81	.5	48.36	.5	44.12	.5	40.15
.6	52.66	.6	48.22	.6	43.99	.6	40.02
.7	52.50	.7	48.07	.7	43.86	.7	39.89
.8	52.36	.8	47.93	.8	43.73	.8	39.76
.9	52.21	.9	47.78	.9	43.60	.9	39.62

TIME ALLOWANCE—Continued

Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance	Rating Measurement	Allowance
89.0	39.50	91.0	36.98	93.0	34.52	95.0	32.16
.1	39.37	.1	36.85	.1	34.40	.1	32.04
.2	39.24	.2	36.72	.2	34.28	.2	31.92
.3	39.11	.3	36.59	.3	34.16	.3	31.80
.4	38.98	.4	36.47	.4	34.04	.4	31.68
.5	38.84	.5	36.35	.5	33.92	.5	31.56
.6	38.71	.6	36.23	.6	33.80	.6	31.44
.7	38.59	.7	36.11	.7	33.68	.7	31.33
.8	38.47	.8	35.99	.8	33.56	.8	31.22
.9	38.35	.9	35.87	.9	33.46	.9	31.12
90.0	38.23	92.0	35.75	94.0	33.35	96.0	31.01
.1	38.10	.1	35.62	.1	33.23	.1	30.89
.2	37.97	.2	35.48	.2	33.11	.2	30.77
.3	37.84	.3	35.36	.3	32.99	.3	30.65
.4	37.70	.4	35.24	.4	32.87	.4	30.53
.5	37.58	.5	35.12	.5	32.75	.5	30.41
.6	37.46	.6	35.00	.6	32.63	.6	30.30
.7	37.34	.7	34.88	.7	32.51	.7	30.19
.8	37.22	.8	34.76	.8	32.39	.8	30.08
.9	37.10	.9	34.64	.9	32.27	.9	29.98

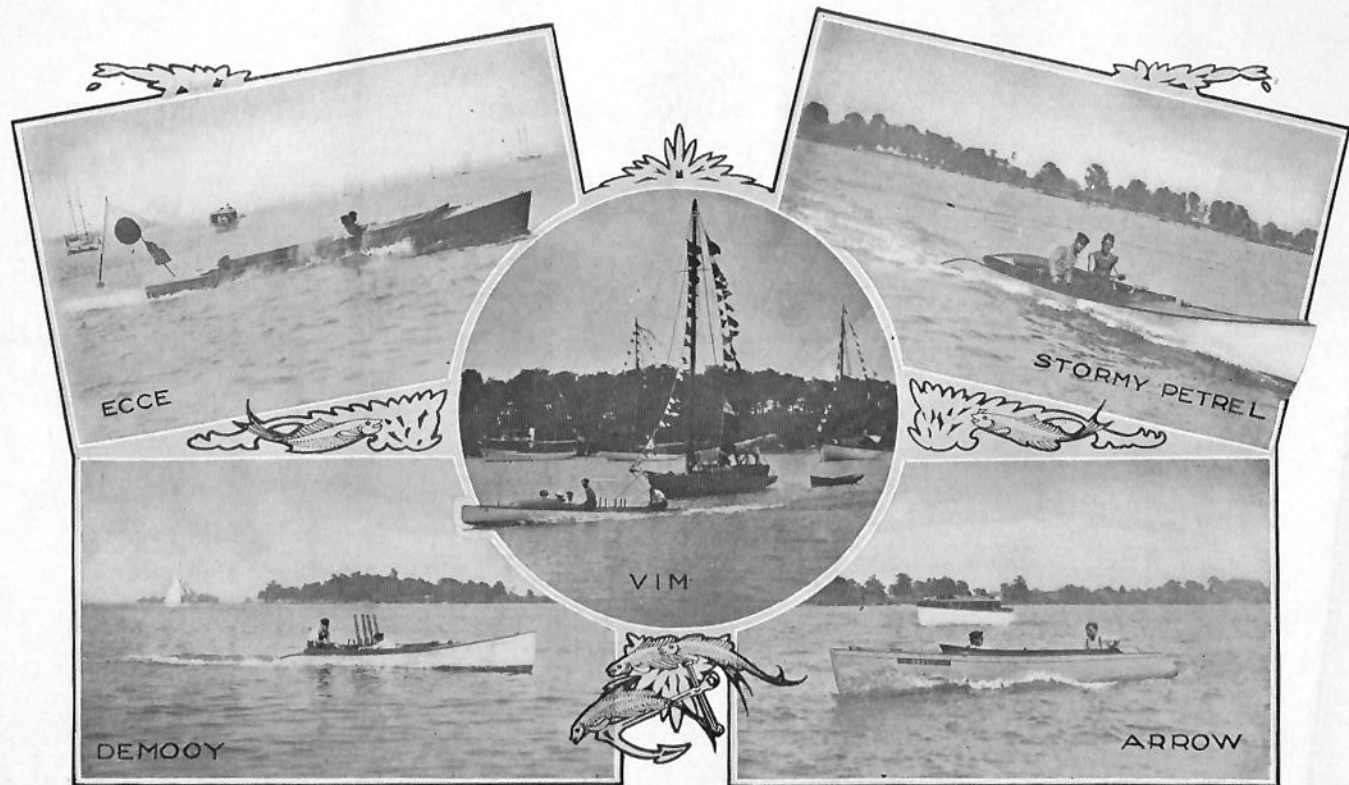


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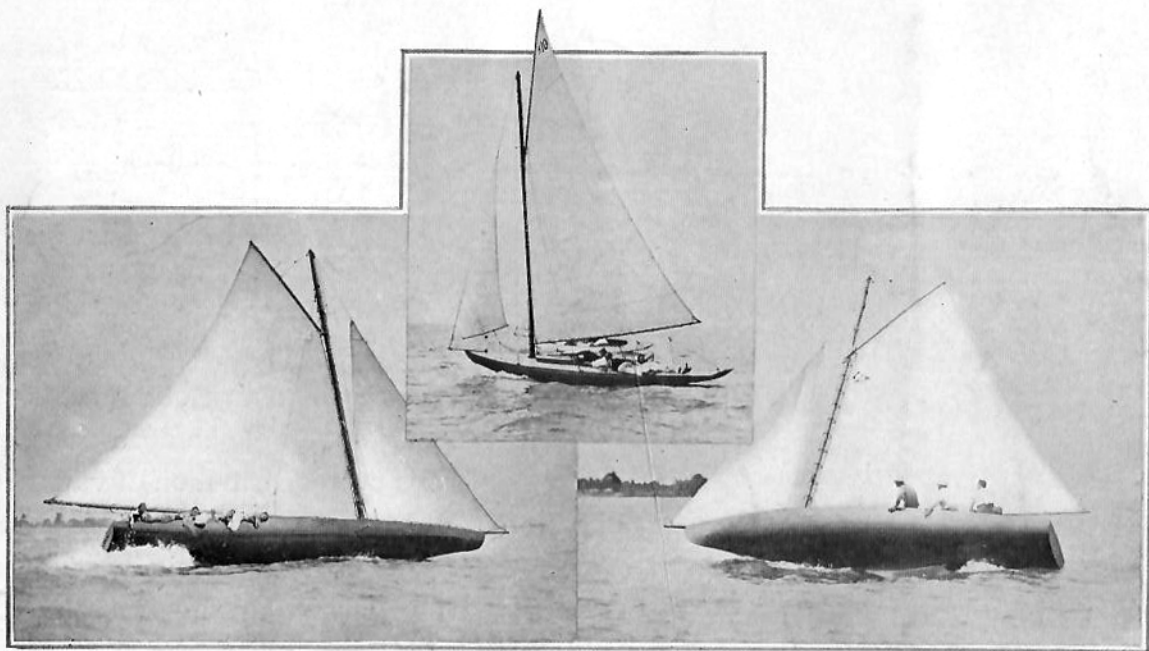
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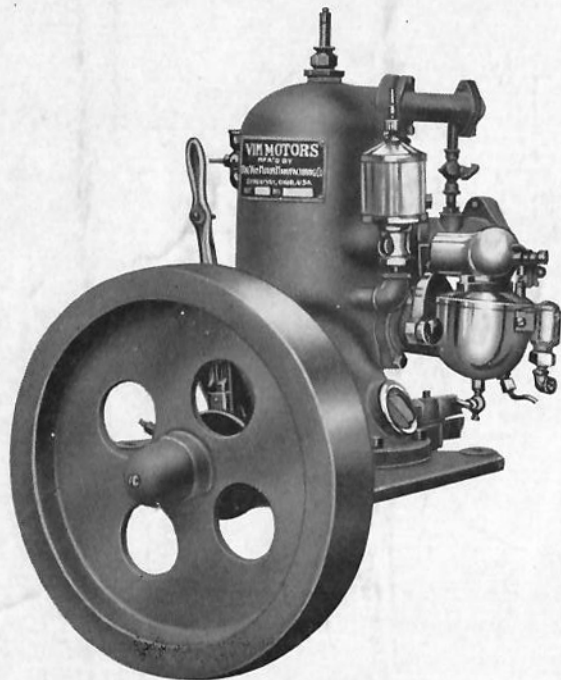
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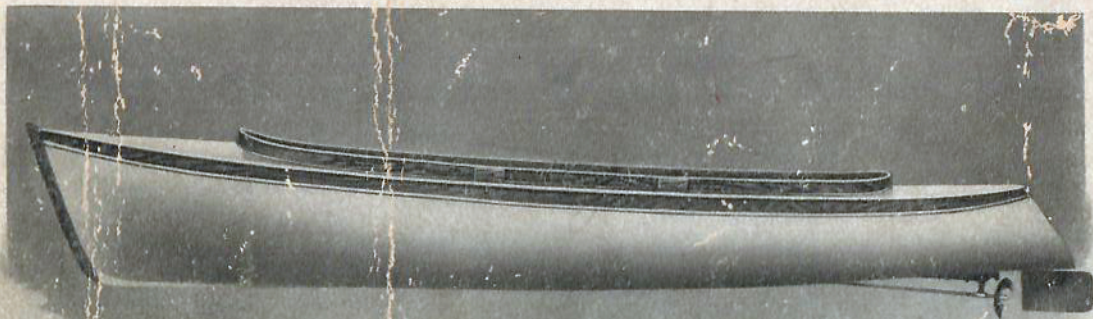
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